

State of New York Executive Department
Office Of General Services
New York State Procurement
Corning Tower Building - 38th Floor
Empire State Plaza
Albany, New York 12242
<http://www.ogs.ny.gov>

REVISED CONTRACT AWARD NOTIFICATION

| | | |
|--------------------------------|----------|--|
| Title | : | Group 31503 – BITUMINOUS CONCRETE HOT MIX ASPHALT VPP (2012 NYSDOT Specific Projects) (Federal & State Funds) |
| | | Classification Code(s): 30 |
| Award Number | : | 22489 |
| Contract Period | : | June 15, 2012 Through September 30, 2013 |
| Bid Opening Date | : | April 26, 2012 |
| Date of Issue | : | June 29, 2012 (Revised April 24, 2013) |
| Specification Reference | : | SPEC 925 dated August 8, 2011 (Supersedes SPEC-920 dated December 15, 2010) and as amended in the Invitation for Bids |
| Contractor Information | : | Appears beginning on Page 2 of this Award |

Address Inquiries To:

| State Agencies & Vendors | Political Subdivisions & Others |
|--|--|
| Name : Elise Relyea Title : Contract Management Specialist Phone : 518-408-2383 Fax : 518-474-1160 E-mail : Elise.Relyea@ogs.ny.gov | New York State Procurement Customer Services Phone : 518-474-6717 Fax : 518-474-2437 E-mail : customer.services@ogs.ny.gov |

**The New York State Procurement values your input.
Complete and return "Contract Performance Report" at end of document.**

Description

Bituminous Concrete is a mixture of stone of various sizes and liquid material. The mixture is heated and proportioned in a bituminous concrete plant and compacted on a road in a heated state. Once the material cools, it becomes a hard durable material. Bituminous concrete is used for preventive maintenance activities which ensure that highways and bridges meet or exceed their optimum useful life.

NOTE: See individual contract items to determine actual awardees.

| CONTRACT # | CONTRACTOR & ADDRESS | TELEPHONE # | FED ID # NYS VID # |
|-------------------|--|--|-------------------------------|
| PC65826 | BARRETT PAVING MATERIALS INC. 26572 NYS Route 37 Watertown, NY 13601 | 315/788-2037 Sylvain Gross Fax: 315/786-0748 E-mail: sgross@barrett paving.com Web Site: barrettindustriescorp.com | 133003901 1000038867 |
| PC65827 SB | BROOME BITUMINOUS PRODUCTS, INC. 416 Prentice Road Vestal, NY 13850 | 607/729-0498 Daniel W. Gates Fax: 607/729-0853 E-mail: danbbpi@stny.rr.com | 161275720 1000015652 |
| PC65833 | HANSON AGGREGATES NY LLC PO Box 513 4800 Jamesville Road Jamesville, NY 13078 | 315/469/5501 Tom McCambley Fax: 315/469-3133 E-mail: tom.mccambley@hanson.com | 160928494 1000028456 |
| PC65838 SB | MILHERST CONSTRUCTION, INC. 10025 County Road Clarence Center, NY 14032-0430 | 716/688-9098 James L. Collins, Jr. Fax: 716/688-9562 E-mail: jimjr@milherst.com Web Site: milherst.com | 161081465 1000015318 |

Cash Discount, If Shown, Should be Given Special Attention.

INVOICES MUST BE SENT DIRECTLY TO THE ORDERING AGENCY FOR PAYMENT.

(See "Contract Payments" and "Electronic Payments" in this document.)

AGENCIES SHOULD NOTIFY THE NEW YORK STATE PROCUREMENT PROMPTLY IF THE CONTRACTOR FAILS TO MEET DELIVERY OR OTHER TERMS OF THIS CONTRACT. PRODUCTS OR SERVICES WHICH DO NOT COMPLY WITH THE SPECIFICATIONS OR ARE OTHERWISE UNSATISFACTORY TO THE AGENCY SHOULD ALSO BE REPORTED TO THE NEW YORK STATE PROCUREMENT.

SMALL, MINORITY AND WOMEN-OWNED BUSINESSES:

The letters SB listed under the Contract Number indicate the contractor is a NYS small business. Additionally, the letters MBE and WBE indicate the contractor is a Minority-owned Business Enterprise and/or Woman-owned Business Enterprise.

NOTE TO AUTHORIZED USERS:

When placing purchase orders under the contract(s), the authorized user should be familiar with and follow the terms and conditions governing its use which usually appears at the end of this document. The authorized user is accountable and responsible for compliance with the requirements of public procurement processes. The authorized user must periodically sample the results of its procurements to determine its compliance. In sampling its procurements, an authorized user should test for reasonableness of results to ensure that such results can withstand public scrutiny.

The authorized user, when purchasing from OGS contracts, should hold the contractor accountable for contract compliance and meeting the contract terms, conditions, specifications, and other requirements. Also, in recognition of market fluctuations over time, authorized users are encouraged to seek improved pricing whenever possible.

Authorized users have the responsibility to document purchases, particularly when using OGS multiple award contracts for the same or similar product(s)/service(s), which should include:

- a statement of need and associated requirements,

- a summary of the contract alternatives considered for the purchase,
- the reason(s) supporting the resulting purchase (e.g., show the basis for the selection among multiple contracts at the time of purchase was the most practical and economical alternative and was in the best interests of the State).

REQUEST FOR CHANGE:

Any request by the agency or contractor regarding changes in any part of the contract must be made in writing to the Office of General Services, New York State Procurement, prior to effectuation.

CONTRACT PAYMENTS:

Payments cannot be processed by State facilities until the contract products have been delivered in satisfactory condition or services have been satisfactorily performed. Payment will be based on any invoice used in the supplier's normal course of business. However, such invoice must contain sufficient data including but not limited to contract number, description of product or service, quantity, unit and price per unit as well as federal identification number.

State facilities are required to forward properly completed vouchers to the Office of the State Comptroller for audit and payment. All facilities are urged to process every completed voucher expeditiously giving particular attention to those involving cash discounts for prompt payment.

If the contract terms indicate political subdivisions and others authorized by law are allowed to participate, those entities are required to make payments directly to the contractor. Prior to processing such payment, the contractor may be required to complete the ordering non-State agency's own voucher form.

See "Contract Billings" in Appendix B, OGS General Specifications.

NOTE TO CONTRACTOR:

This Contract Award Notification is not an order. Do not take any action under this contract except on the basis of purchase order(s) from the agency or agencies.

If a purchase order is not received from New York State Department of Transportation within two weeks after receipt of this award, the contractor should contact the agency directly requesting the submission of a purchase order. The Agency contact person is Hasib Khan, who can be reached 518/457-1572.

NEW YORK STATE VENDOR RESPONSIBILITY QUESTIONNAIRE FOR-PROFIT BUSINESS ENTITY:

Contractor is encouraged to maintain up-to-date Questionnaire during the life of the contract and is also required to ensure this Questionnaire reflects any substantive issues that may have occurred from the time the Contract was initially awarded.

DEBRIEFING:

Contractors and bidders are accorded fair and equal treatment with respect to the opportunity for debriefing. OGS shall, upon request, provide a debriefing to any bidder or awarded contractor that responded to the IFB or RFP regarding the reason that the proposal or bid submitted by the unsuccessful bidder was not selected for a contract award. The post award debriefing should be requested by the bidder or awarded contractor within thirty days of posting of the contract award on the OGS website.

NYS PRO's DISPUTE RESOLUTION POLICY:

It is the policy of the Office of General Services' New York State Procurement (NYS PRO) to provide vendors with an opportunity to administratively resolve disputes, complaints or inquiries related to NYS PRO bid solicitations or contract awards. NYS PRO encourages vendors to seek resolution of disputes through consultation with NYS PRO staff. All such matters will be accorded impartial and timely consideration. Interested parties may also file formal written disputes. A copy of NYS PRO's Dispute Resolution Procedures for Vendors may be obtained by contacting the person shown on the front of this document or through the OGS website (www.ogs.ny.gov).

CONTRACT BILLINGS AND PAYMENTS:

a. Billings. Contractor and the dealers/distributors/resellers designated by the Contractor, if any, shall provide complete and accurate billing invoices to each Authorized User in order to receive payment. Billing invoices submitted to an Authorized User must contain all information required by the Contract and the State Comptroller or other appropriate fiscal officer. Submission of an invoice and payment thereof shall not preclude the Commissioner from requesting reimbursement or demanding a price adjustment in any case where the Product delivered is found to deviate from the terms and conditions of the Contract or where the billing was inaccurate.

Contractor shall provide, upon request of the Commissioner, any and all information necessary to verify the accuracy of the billings. Such information shall be provided in the format requested by the Commissioner and in a media commercially available from the Contractor. The Commissioner may direct the Contractor to provide the information to the State Comptroller or to any Authorized User of the Contract.

b. Payment of Contract purchases made by an Authorized User when the State Comptroller is responsible for issuing such payment. The Authorized User and Contractor agree that payments for invoices submitted by the Contractor shall only be rendered electronically unless payment by paper check is expressly authorized by the Commissioner, in the Commissioner's sole discretion, due to extenuating circumstances. Such electronic payments shall be made in accordance with ordinary State procedures and practices. The Contractor shall comply with the State Comptroller's procedures to authorize electronic payments. Authorization forms are available at the State Comptroller website at www.osc.state.ny.us, by e-mail at epunit@osc.state.ny.us, or by telephone at 518-486-1255. Contractor acknowledges that it will not receive payment on any invoices submitted under this Contract that are payable by the State Comptroller if it does not comply with the State Comptroller's electronic payment procedures, except where the Commissioner has expressly authorized payment by paper check as set forth above.

c. Payment of Contract purchases made by an Authorized User when the State Comptroller is not responsible for issuing such payment. The Authorized User and Contractor agree that payments for such Contract purchases shall be billed directly by Contractor on invoices/vouchers, together with complete and accurate supporting documentation as required by the Authorized User. Such payments shall be as mandated by the appropriate governing law from the receipt of a proper invoice. Such Authorized User and Contractor are strongly encouraged to establish electronic payments.

DIESEL EMISSION REDUCTION ACT OF 2006:

Pursuant to the Diesel Emissions Reduction Act, §19-0323 of the N.Y. Environmental Conservation Law ("NYECL"), it is now a requirement that heavy duty diesel vehicles in excess of 8,500 pounds use the best available retrofit technology ("BART") and ultra low sulfur diesel fuel ("ULSD"). The requirement of the Law applies to all vehicles owned, operated by or on behalf of, or leased by State agencies and State or regional public authorities. These vehicles are to be operated exclusively on ULSD. It also requires that such vehicles owned, operated by or on behalf of, or leased by State agencies and State or regional public authorities with more than half of its governing body appointed by the Governor utilize BART.

The Law may be applicable to vehicles used by contract vendors "on behalf of" State agencies and public authorities. The Law provides a list of exempted vehicles. Regulations provide further guidance as to the effects of the Law on contract vendors using heavy duty diesel vehicles on behalf of the State. The Law also permits waivers of ULSD and BART under limited circumstances at the discretion of the Commissioner of Environmental Conservation. The Law will also require reporting from State agencies and from contract vendors in affected contracts.

Therefore, the contractor hereby certifies and warrants that all heavy duty vehicles, as defined in NYECL §19-0323, to be used under this contract, will comply with the specifications and provisions of NYECL §19-0323, and any regulations promulgated pursuant thereto, which requires the use of BART and ULSD, unless specifically waived by NYSDEC. Qualification and application for a waiver under this Law will be the responsibility of the contractor.

MERCURY-ADDED CONSUMER PRODUCTS:

Offerers are advised that effective January 1, 2005, Article 27, Title 21 of the Environmental Conservation Law bans the sale or distribution free of charge of fever thermometers containing mercury except by prescription written by a physician and bans the sale or distribution free of charge of elemental mercury other than for medical pre-encapsulated dental amalgam, research, or manufacturing purposes due to the hazardous waste concerns of mercury. The law further states that effective July 12, 2005, manufacturers are required to label mercury-added consumer products that are sold or offered for sale in New York State by a distributor or retailer. The label is intended to inform consumers of the presence of mercury in such products and of the proper disposal or recycling of mercury-added consumer products. Offerers are encouraged to contact the Department of Environmental Conservation, Bureau of Solid Waste, Reduction & Recycling at (518) 402-8705 or the Bureau of Hazardous Waste Regulation at 1-800-462-6553 for questions relating to the law. Offerers may also visit the Department's web site for additional information:
<http://www.dec.ny.gov/chemical/8512.html>.

IRAN DIVESTMENT ACT

As a result of the Iran Divestment Act of 2012 (Act), Chapter 1 of the 2012 Laws of New York, a new provision has been added to the State Finance Law (SFL), § 165-a, effective April 12, 2012. Under the Act, the Commissioner of the Office of General Services (OGS) will be developing a list (prohibited entities list) of "persons" who are engaged in "investment activities in Iran" (both are defined terms in the law). Pursuant to SFL § 165-a(3)(b), the initial list is expected to be issued no later than 120 days after the Act's effective date, at which time it will be posted on the OGS website.

By submitting a bid in response to this solicitation or by assuming the responsibility of a Contract awarded hereunder, Bidder/Contractor (or any assignee) certifies that once the prohibited entities list is posted on the OGS website, it will not utilize on such Contract any subcontractor that is identified on the prohibited entities list.

Additionally, Bidder/Contractor is advised that once the list is posted on the OGS website, any Contractor seeking to renew a Contract or assume the responsibility of a Contract awarded in response to the solicitation, certifies at the time the Contract is renewed or assigned that it or its assignee is not included on the prohibited entities list.

During the term of the Contract, should OGS receive information that a person is in violation of the above-referenced certification, OGS will offer the person an opportunity to respond. If the person fails to demonstrate that it has ceased its engagement in the investment which is in violation of the Act within 90 days after the determination of such violation, then OGS shall take such action as may be appropriate including, but not limited to, imposing sanctions, seeking compliance, recovering damages, or declaring the Contractor in default.

OGS reserves the right to reject any bid or request for assignment for an entity that appears on the prohibited entities list prior to the award of a contract, and to pursue a responsibility review with respect to any entity that is awarded a contract and appears on the prohibited entities list after contract award.

CONTRACTOR REQUIREMENTS AND PROCEDURES FOR EQUAL EMPLOYMENT AND BUSINESS PARTICIPATION OPPORTUNITIES FOR MINORITY GROUP MEMBERS AND NEW YORK STATE CERTIFIED MINORITY- AND WOMEN-OWNED BUSINESS ENTERPRISES

POLICY STATEMENT

The New York State Office of General Services (OGS), as part of its responsibility, recognizes the need to promote the employment of minority group members and women and to ensure that certified minority and women-owned business enterprises have opportunities for maximum feasible participation in the performance of OGS contracts.

In 2006, the State of New York commissioned a disparity study to evaluate whether minority and women-owned business enterprises had a full and fair opportunity to participate in state contracting. The findings of the study were published on April 29, 2010, under the title "The State of Minority and Women-Owned Business Enterprises: Evidence from New York" ("the Disparity Study"). The report found evidence of statistically significant disparities between the level of participation of minority and women-owned business enterprises in state procurement contracting verses the number of minority and women-owned business enterprises that were ready, willing and able to participate in state procurements. As a result of these findings, the Disparity Study made recommendations concerning the implementation and operation of the statewide certified minority and women-owned business enterprises program.

EQUAL EMPLOYMENT OPPORTUNITY REQUIREMENTS

Contractor agrees with all of the terms and conditions of Appendix A including Clause 12 - Equal Employment Opportunities for Minorities and Women. The contractor is required to ensure that it and any subcontractors awarded a subcontract over \$25,000 for the construction, demolition, replacement, major repair, renovation, planning or design of real property and improvements thereon (the "Work") except where the Work is for the beneficial use of the Contractor, shall undertake or continue programs to ensure that minority group members and women are afforded equal employment opportunities without discrimination because of race, creed, color, national origin, sex, age, disability or marital status. For these purposes, equal opportunity shall apply in the areas of recruitment, employment, job assignment, promotion, upgrading, demotion, transfer, layoff, termination, and rates of pay or other forms of compensation. This requirement does not apply to: (i) work, goods, or services unrelated to this contract; or (ii) employment outside New York State.

Contractor will, upon request, submit to OGS a workforce utilization report (Form EEO 101) identifying the workforce actually utilized on the Contract if known.

Further, pursuant to Article 15 of the Executive Law (also known as the Human Rights Law) and all other State and Federal statutory and constitutional non-discrimination provisions, the Contractor and sub-contractors will not discriminate against any employee or applicant for employment because of race, creed (religion), color, sex (including gender expression), national origin, sexual orientation, military status, age, disability, predisposing genetic characteristic, marital status or domestic violence victim status, and shall also follow the requirements of the Human Rights Law with regard to non-discrimination on the basis of prior criminal conviction and prior arrest.

Business Participation Opportunities for New York State Certified Minority- and Women-Owned Business Enterprises (MWBE).

For purposes of this procurement, OGS has conducted a comprehensive search and has determined that the contract does not offer any opportunities for participation by MWBEs.

ALL FORMS ARE AVAILABLE AT <http://www.ogs.ny.gov/MWBE/Forms.asp> .

CONTRACTOR INSURANCE:

The Contractor shall provide to The New York State Office of General Services (“OGS”) written proof of insurance coverage and additional insured documentation as specified herein. "Written proof" consists of certificates of insurance and/or endorsements to policies issued by an officer of an insurance company licensed or authorized to do business in New York, government self-retention funds or other self-insurance companies evidencing that the Contractor has the requisite insurance coverages. All non-standard exclusions or limitations applicable to the contract must be disclosed on the Certificate of Insurance and must be approved by The New York State Office of General Services (“OGS”). Policies providing commercial general liability, excess or umbrella liability and pollution legal liability insurance shall be specifically endorsed to name the People of the State of New York, its officers, agents, and employees as additional insureds thereunder. Such written proof shall be in the form and substance acceptable to The New York State Office of General Services (“OGS”). Acceptance and/or approval by The New York State Office of General Services (“OGS”) of the written proof of insurance does not and shall not be construed to relieve Contractor of any obligations, responsibilities or liabilities under the Contract to obtain the required coverage.

Contractor shall secure and continue to keep in force during the term of the contract, and Contractor shall require all Subcontractors prior to commencement of an agreement between Contractor and the Subcontractor, to secure and keep in force during the term of this contract the following insurance coverage in parenthesis:

- a) **Commercial General Liability Insurance** with minimum liability limits of **not less than \$2,000,000.00 each occurrence** (and minimum liability limits of not less than \$5,000,000 after notice of award). Such liability shall be written on the ISO occurrence form CG 00 01© (current edition) or a substitute form providing equivalent coverages and shall cover liability arising from premises or operations, independent Contractors, broad form property damage, personal & advertising injury, cross liability coverage, contractual damages, and products or completed operations, if applicable (including the tort liability of another assumed in a contract), and explosion, collapse & underground coverage. If such insurance contains an aggregate limit, it shall apply separately on a per job basis. General Liability Additional Insured Endorsement shall be on Insurance Service Office’s (ISO) form number **CG 2010 1185**.
- b) **Comprehensive Business Automobile Liability Insurance** with minimum liability limits of **not less than \$2,000,000.00** each accident both at the time of bid and after notice of award. Such insurance shall cover liability arising out of any automobile including Owned (if any), Hired and Non-Owned automobiles.
- c) **Workers’ Compensation, Employer’s Liability, and Disability Benefits** meeting all New York State statutory requirements. The policy shall provide coverage for all states of operation that apply to the performance of the contract. In addition, if employees will be working on, near or over navigable waters, coverage provided under the US Longshore and Harbor Workers Compensation Act must be included. Also, if the contract is for temporary staffing services or involves renting equipment with operators, the Alternate Employer Endorsement WC 00 03 01A must be included on the policy naming the People of the State of New York as the alternate employer.
- d) **OCP Insurance*** - Owners and Contractors Protective Insurance Coverage (OCP) is required by the New York Department of Transportation (NYSDOT) on any NYSDOT project on or near active highway right of way.

The OCP insurance required shall be \$1,000,000 per occurrence and \$2,000,000 in the aggregate.

* Contractor shall provide written proof of such coverage to the Authorized User prior to commencement of work at the Pre-paving/Pre-production conference

CONTRACTOR INSURANCE: (Cont'd)

All insurance coverage must meet the following additional requirements:

- 1) All insurance required shall be obtained at the sole cost and expense of the Contractor, and shall be primary and non-contributing to any insurance, self-retention or self-insurance maintained by the Authorized User.
- 2) Any deductible or self-insured retention amount or other similar obligation under the policies shall be the sole responsibility of the Contractor. The amount of any deductible or self-insured retention is subject to approval by The New York State Office of General Services ("OGS").
- 3) The requisite insurance may be provided through a policy or policies of insurance which may be primary and/or excess including umbrella policies, but must be placed with an Insurer rated "A-" Class "VII" or better by the A.M. Best Company, Inc. If, during the term of the policy, an Insurer's rating falls below "A-" Class "VII", the insurance must be replaced no later than the renewal date of the policy with an Insurer rated at least "A-" Class "VII" by the A.M. Best Company, Inc. Any excess policy must follow the requirements set forth in the New York State Insurance Law for such coverage.
- 4) Contractors shall provide The New York State Office of General Services ("OGS") with updated Certificates of Insurance and as applicable amendatory endorsements at least thirty (30) days prior to the expiration or renewal date of a policy.
- 5) The insurance provided shall include an endorsement indicating that the policy and any endorsements may not be cancelled without thirty (30) days prior written notice to The New York State Office of General Services ("OGS"). In the event that the cancellation is due to non-payment of premium, ten (10) days prior written notice shall be provided.
- 6) The insurance provided shall include a blanket or specific "Waiver of Subrogation" endorsement waiving any right to recover the insurance company may have against the State.
- 7) In the block provided in the Certificate of Insurance for insertion of "Description of Operations/Locations/Vehicles/Exclusions Added by Endorsement/Special Provisions" the additional insured information and job-specific information such as the nature of the contract and either the solicitation number or the contract award number should be referenced.
- 8) Additional insured endorsements should specify the following:
 - The full legal name of the additional insured; i.e., the State of New York, its agencies, officers and employees;
 - The specific location or operations for which the coverage applies;
 - Coverage will run until the completion of the last project on this contract;
 - That notice of modification or cancellation will be provided to the additional insured at a specified name and address;
 - That the insurance company waives any right of recovery it may have against the State;
 - That the coverage required shall be primary for the State and shall not be affected by any self-insurance or other insurance or coverage obtained by the State on its own behalf;
 - That cross-liability/severability of interest coverage is provided; and
 - That the legal defense provided to the State under the policy must be free of any conflicts of interest even if retention of separate legal counsel for the State is necessary.
- 9) The insolvency or bankruptcy of the insured Contractor or Subcontractor shall not release the Insurer from payment under the policy even when such insolvency or bankruptcy prevents the insured Contractor or Subcontractor from meeting the retention limits under the policy.

Failure to provide insurance coverage as required herein and to keep the same in force during the term of the contract is a material breach of contract entitling the State to terminate the contract in accordance with the termination provisions in the contract.

In addition, as stated above, Workers' Compensation, Employer's Liability, and Disability Benefits coverage meeting all New York State statutory requirements are required.

EMERGENCY PURCHASING:

In the event that a disaster emergency is declared by Executive Order under Section 28 of Article 2-B of the Executive Law, or that the Commissioner determines pursuant to his/her authority under Section 163(10)(b) of the State Finance Law that an emergency exists requiring the prompt and immediate delivery of products or services, the Commissioner reserves the right to obtain such products or services from any source, including but not limited to this contract, as the Commissioner in his/her sole discretion determines will meet the needs of such emergency. Contractor shall not be entitled to any claim or lost profits for products or services procured from other sources pursuant to this paragraph.

CONTRACT PERIOD AND RENEWALS:

It is the intention of the State to enter into a contract for the term as stated herein except that the commencement and termination dates may be adjusted forward unilaterally by the State for any resulting contract for up to two calendar months, by indicating such change on the Contract Award Notification.

The contract dates may be adjusted forward beyond two months only with the approval of the Contractor. If, however, the Contractor is not willing to accept an adjustment of the contract dates beyond the two month period, the State reserves the right to proceed with an award to another bidder.

If mutually agreed between the New York State Procurement and the Contractor, the contract may be renewed under the same terms and conditions for additional period(s) not to exceed a total contract term of five (5) years.

SHORT TERM EXTENSION:

Any contract let and awarded hereunder by the State, may be extended unilaterally by the State for an additional period of up to one month upon notice to the Contractor with the same terms and conditions as the original contract including, but not limited to, quantities (prorated for such one month extension), prices, and delivery requirements. With the concurrence of the Contractor, the extension may be for a period of up to three months in lieu of one month. However, this extension terminates should the replacement contract be issued in the interim.

CANCELLATION FOR CONVENIENCE

The State of New York retains the right to cancel this contract, in whole or in part without reason provided that the Contractor is given at least sixty (60) days notice of its intent to cancel. This provision should not be understood as waiving the State's right to terminate the contract for cause or stop work immediately for unsatisfactory work, but is supplementary to that provision. Any such cancellation shall have no effect on existing Agency agreements, which are subject to the same 60 day discretionary cancellation or cancellation for cause by the respective user Agencies.

USE OF RECYCLED OR REMANUFACTURED MATERIALS:

New York State, as a member of the Council of Great Lakes Governors, supports and encourages Vendors to use recycled, remanufactured or recovered materials in the manufacture of products and packaging to the maximum extent practicable without jeopardizing the performance or intended end use of the product or packaging unless such use is precluded due to health or safety requirements or product specifications contained herein. Refurbished or remanufactured components or products are required to be restored to original performance and regulatory standards and functions and are required to meet all other requirements of this bid solicitation. Warranties on refurbished or remanufactured components or products must be identical to the manufacturer's new equipment warranty or industry's normal warranty when remanufacturer does not offer new equipment. See "Remanufactured, Recycled, Recyclable or Recovered Materials" in Appendix B, OGS General Specifications.

SCOPE:

Bituminous Concrete is a mixture of stone of various sizes and liquid material. The mixture is heated and proportioned in a bituminous concrete plant and compacted on a road in a heated state. Once the material cools, it becomes a hard durable material. Bituminous concrete is used for preventive maintenance activities which ensure that highways and bridges meet or exceed their optimum useful life.

PRICE:

General - Clause 24B of the General Specifications has been modified to read:

Price is net per ton, furnished, delivered, dumped into approved spreading machines, placed, and compacted totally by the Contractor. The existing bituminous concrete surface (and any surfaces included in this contract that will be overlaid by this contract) shall be treated with tack coat. **Tack coat shall be paid under its own item and price is net per gallon and includes furnishing, delivering, and applying the tack coat as indicated.** Price adjustments, if any, will be calculated on the basis of the material actually furnished.

The vendor is to furnish all necessary labor and equipment to complete the indicated projects except that the State will supervise and control the operation. Permanent pavement striping will be the responsibility of the State upon completion of the paving after the vendor has vacated the project site. The equipment supplied to place the hot mix asphalt shall meet the requirements of Section 402 of the New York State Department of Transportation Standard Specifications. The equipment supplied to place the tack coat shall meet the requirements of Section 407 of the New York State Department of Transportation Standard Specifications.

Hot mix asphalt pavers shall meet the requirements of Sub-Section 402-3.02, Hot Mix Pavers, of the New York Department of Transportation Standard Specifications. Compaction equipment shall meet the requirements of Sub-Section 402-3.04, Rollers of the Specification. All necessary operators shall be supplied along with the hot mix asphalt paver, rollers and distributor.

The approved hot mix asphalt pavers shall be capable of simultaneously paving the travel lanes and the shoulders as indicated in the Project Dimensions Table. All personnel supplied for the paving shall be qualified and experienced in hot mix asphalt paving.

Insurance - Price includes ALL required insurance coverage costs. In particular, price includes the following:

- Commercial General Liability Insurance with a limit of not less than \$5,000,000 each occurrence ;
- Comprehensive Business Automobile Liability Insurance with a limit of not less than \$2,000,000 each accident;
- Owners and Contractors Protective Insurance Coverage (OCP) with a limit of not less than \$1,000,000 per occurrence and \$2,000,000 in the aggregate.

Each requirement should be reviewed carefully. (Please see separate CONTRACTOR INSURANCE clause)

ASPHALT PRICE ADJUSTMENTS:

1. Asphalt price adjustments allowed will be based on the **March 1, 2012** average of the F.O.B. terminal price **per ton** of unmodified PG 64-22 binder without anti-stripping agent (base average F.O.B. terminal price) **for the hot mix asphalt and tack coat.**

The March 1, 2012 average is \$613.00 per ton.

The new monthly average terminal price will be determined by the New York State Department of Transportation based on prices of pre-approved primary sources of performance graded binder in accordance with the New York State Department of Transportation Standard Specification.

NOTE: The same grade of asphalt cement used in establishing the base average F.O.B. terminal price shall be used in establishing the new average F.O.B. terminal price.

In the event that one or more of the New York State Department of Transportation pre-approved sources discontinue posting a price for asphalt cement, the base average F.O.B. terminal **price shall not be recalculated.**

2. The new average F.O.B. terminal price will be determined based on the above F.O.B. terminal prices posted on the 20th of each month, hereafter known as the "Adjustment Date", during the contract period. However, asphalt price adjustments, in accordance with the formula below, will be effective for deliveries made on and after the first of the month following the adjustment date.

PRICE: (Cont'd)

ASPHALT PRICE ADJUSTMENTS: (Cont'd)

3. The unit prices per ton of hot mix asphalt (HMA) and per gallon of tack coat purchased from any award based on this specification will be subject to adjustment based on the following formulae:

Hot Mix

| | | | | |
|----------------------------|---|---|---|-------------------------------------|
| Price Adjustment (per ton) | = | $\left(\frac{\text{New Monthly Average FOB Terminal Price} - \text{Base Average FOB Terminal Price}}{235} \right)$ | X | Total % Asphalt Plus Fuel Allowance |
|----------------------------|---|---|---|-------------------------------------|

Tack Coat

| | | | | |
|-------------------------------|---|--|---|-------------------------------------|
| Price Adjustment (per gallon) | = | $\frac{\text{New Monthly Average FOB Terminal Price} - \text{Base Average FOB Terminal Price}}{235}$ | X | Total % Asphalt Plus Fuel Allowance |
|-------------------------------|---|--|---|-------------------------------------|

NEW MONTHLY AVERAGE F.O.B. TERMINAL PRICE:

The average F.O.B. terminal price for unmodified PG 64-22 binder without anti-stripping agent is as determined by the New York State Department of Transportation per New York State Department of Transportation Standard Specification.

BASE AVERAGE F.O.B. TERMINAL PRICE:

The average F.O.B. terminal price of unmodified PG 64-22 binder without anti-stripping agent is as determined by the New York State Department of Transportation as of **March 1, 2012**.

TOTAL % ASPHALT PLUS FUEL:

The percentage of total allowable asphalt and fuel for each item is as follows:

| Item | % Asphalt | +Fuel Allowance | Total % Asphalt Plus Fuel |
|------------------------|-----------|-----------------|---------------------------|
| 402.017902 | **** | 1 | **** |
| 402.018902 | **** | 1 | **** |
| 402.058902 | 8.25 | 1 | 9.25% |
| 402.068X0118 | 6.70 | 1 | 7.70% |
| 402.09XX02 | 6.20 | 1 | 7.20% |
| 402.12XX02 | 5.50 | 1 | 6.50% |
| 402.19XX02 | 4.90 | 1 | 5.90% |
| 407.0101 Tack Coat | 40.00 | 0.2 | 40.20% |
| 407.02010018 Tack Coat | 55.00 | 0.2 | 55.20% |

****The conversion factor for Truing & Leveling will be computed separately using the conversion factors for the individual mixtures used.

| | |
|-----------------------------------|---|
| EXAMPLE: Item 402.09XX02 | $\frac{\text{New Avg. Price} - \text{Base Avg. Price}}{235} \times \text{Total \% Asphalt Plus Fuel} = \text{Price Adjustment}$ |
| Base Avg. Price = \$613.000 | (\$650.000 - \$613.000) |
| New Avg. Price = \$650.000 | X 0.072 = +\$2.664 per ton |
| Total % Asphalt Plus Fuel = 7.20% | |

| | |
|---|---|
| EXAMPLE: Item 407.0101 Tack Coat | $\frac{\text{New Avg. Price} - \text{Base Avg. Price}}{235} \times \text{Total \% Asphalt Plus Fuel} = \text{Price Adjustment}$ |
| Base Avg. Price = \$613.000 | \$650.000 - \$613.000 |
| New Avg. Price = \$650.000 | 235 X 0.402 = +\$0.063 per gallon |
| Total % Asphalt Plus Fuel = 40.20% | |

+ Fuel Allowance represents allowance for energy (fuel, electricity, natural gas) used in the production of asphalt. It is a cost associated with the product and not intended to represent any trucking or hauling of product.

Positive Price Adjustment number shall be added to original per ton/gallon Bid Price.

Negative Price Adjustment number shall be subtracted from original per ton/gallon Bid Price.

PRICE: (Cont'd)

ASPHALT PRICE ADJUSTMENTS: (Cont'd)

4. Work performed after the expiration of the contract, where no extension has been granted, resultant from purchase orders placed prior to expiration of the contract will receive the asphalt price adjustments applicable in effect during the last month of the contract.

Asphalt price adjustments for any contracts that are extended will be based on the new average for the month in which the work is done applying the same base established for that contract.
5. Asphalt price adjustments allowed by this contract shall be calculated and applied to the original prices. There will not be asphalt price adjustments unless the change amounts to more than \$0.10 per ton from the original price for the hot mix and \$0.0150 per gallon for the tack coat. In these instances, prices will revert back to the original prices.
6. All asphalt price adjustments will be computed to three decimal places.
7. Should these provisions result in a price structure which becomes unworkable, detrimental or injurious to the State or in prices which are not truly reflective of market conditions or which are deemed by the Commissioner to be unreasonable or excessive, and no adjustment in price is mutually agreeable, the Commissioner reserves the sole right upon ten days written notice mailed to the Contractor to terminate any contract resulting from this bid opening.
8. All asphalt price adjustments shall be published by the State and issued to all contract holders whose responsibility will be to attach the appropriate State notification (based on when the work was performed) to the payment invoice submitted to agency.

U.S. CUSTOMARY UNITS:

All construction and materials quantities specified are in U.S. Customary units, and should be billed accordingly.

NYSDOT STANDARD SPECIFICATIONS:

References are made herein to New York State Department of Transportation, Standard Specifications, Construction and Materials, dated May 1, 2008 and all current addenda. A copy may be obtained through the Department's publication unit. Call 518-457-4401 for information.

APPROVED FACILITY AND INSPECTION:

For information regarding how to become an approved facility, or, to make arrangements for inspection of materials or equipment when required, contact the Materials Bureau of the Department of Transportation at 518/457-3240.

ESTIMATED QUANTITIES:

The quantities or dollar values listed are estimated only. See "Estimated/Specific Quantity Contracts" in Appendix B, OGS General Specifications.

DELIVERY:

General - Delivery shall be made in accordance with instructions on Purchase Order from agency. If there is a discrepancy between the purchase order and what is listed on the contract, it is the contractor's obligation to seek clarification from the ordering agency and, if applicable, from the Office of General Services, New York State Procurement.

Default (Failure to Furnish Material) - Clause 45 of the General Specifications is modified as follows: Failure to furnish material within ten days after receiving order or as agreed upon with authorized representative or violation of shipping instructions, shall be cause for and entitle the State (1) to damages which in its judgment have resulted, or (2) to purchase in the open market at the expense of the contractor. At the discretion of the State, one or both of these courses of action may be followed.

Delivery Ticket - A delivery ticket shall be provided with each load of bituminous material and filler for joints stating the following:

1. Storage facility identification
2. Ticket Number
3. Date/time
4. Item Number and Type
5. Quantity ticket printed by machine
6. Quantity in 60° F gallons for emulsions and PG binder.

Quantity Received - It is the responsibility of the agency to ascertain quantities shipped are accurate to the delivery ticket. Each vehicle should be checked for product upon arrival and prior to departure.

PAYMENT:

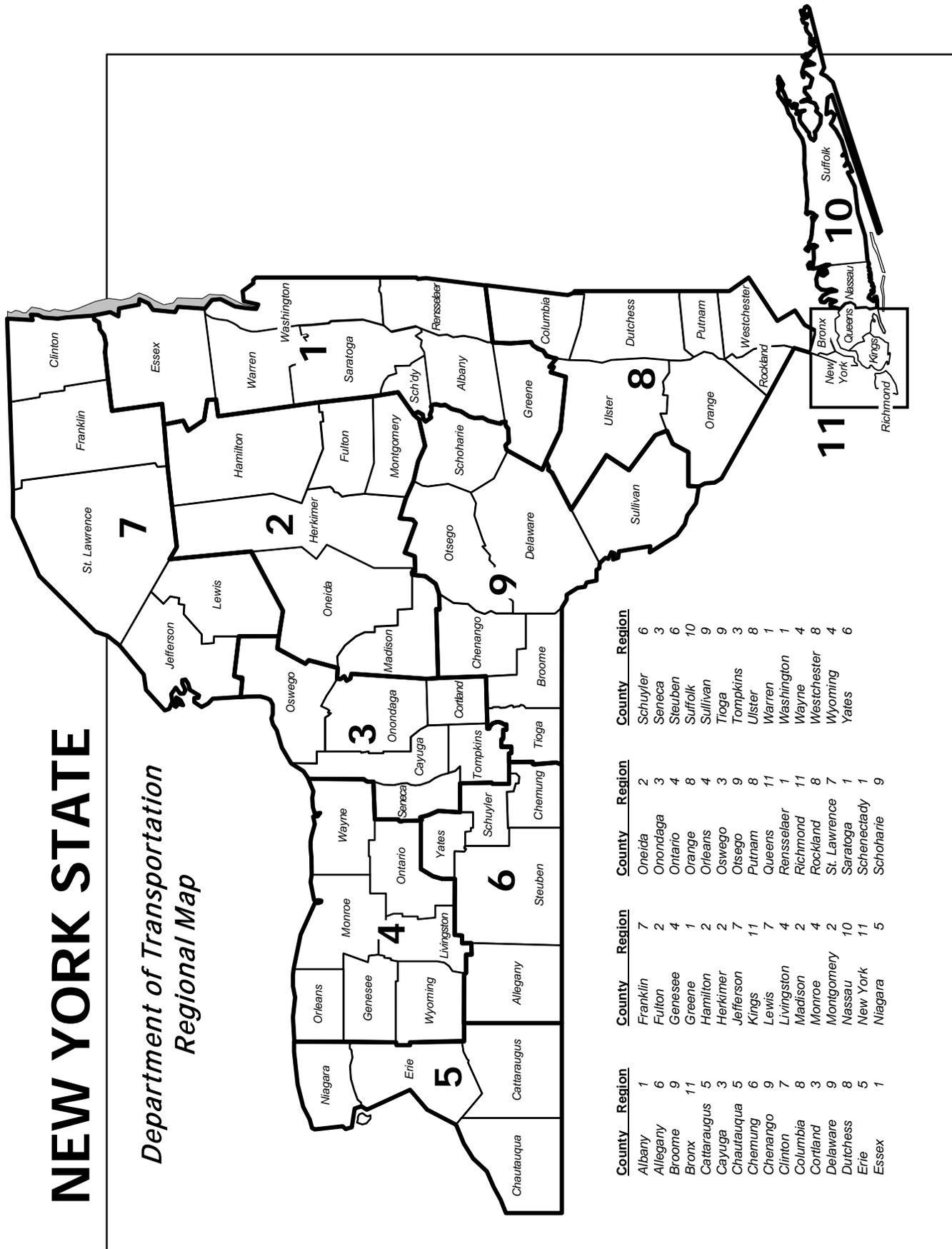
Payment shall be made at contract prices per net ton for the actual quantity of material placed by the Contractor.
Payment shall be made at the contract price per gallon for the actual quantity of tack coat placed by the Contractor.
Payment adjustments will be applied in Quality Units for all applicable mixes as described in Sub-Section 401-4 and 402-4.
The following Index Prices shall be used for all projects contained in this contract:

QUALITY UNIT INDEX PRICES

| <u>Region</u> | <u>Index Price (\$/Quality Unit)</u> |
|---------------|--|
| 1 | \$65 |
| 2 | \$70 |
| 3 | \$80 |
| 4 | \$70 |
| 5 | \$75 |
| 6 | \$65 |
| 7 | \$70 |
| 8 | \$85 |
| 9 | \$65 |
| 10 | \$110 |
| 11 | \$110 |

NEW YORK STATE

Department of Transportation Regional Map



| County | Region | County | Region | County | Region | County | Region |
|-------------|--------|------------|--------|--------------|--------|-------------|--------|
| Albany | 1 | Franklin | 7 | Oneida | 2 | Schuyler | 6 |
| Allegany | 6 | Fulton | 2 | Onondaga | 3 | Seneca | 3 |
| Broome | 9 | Genesee | 4 | Ontario | 4 | Steuben | 6 |
| Bronx | 11 | Greene | 1 | Orange | 8 | Suffolk | 10 |
| Cattaraugus | 5 | Hamilton | 2 | Orleans | 4 | Sullivan | 9 |
| Cayuga | 3 | Herkimer | 2 | Oswego | 3 | Tioga | 9 |
| Chautauqua | 5 | Jefferson | 7 | Otsego | 9 | Tompkins | 3 |
| Chemung | 6 | Kings | 11 | Putnam | 8 | Ulster | 8 |
| Chenango | 9 | Lewis | 7 | Queens | 11 | Warren | 1 |
| Clinton | 7 | Livingston | 4 | Rensselaer | 1 | Washington | 1 |
| Columbia | 8 | Madison | 2 | Richmond | 11 | Wayne | 4 |
| Cortland | 3 | Monroe | 4 | Rockland | 8 | Westchester | 8 |
| Delaware | 9 | Montgomery | 2 | St. Lawrence | 7 | Wyoming | 4 |
| Dutchess | 8 | Nassau | 10 | Saratoga | 1 | Yates | 6 |
| Erie | 5 | New York | 11 | Schenectady | 1 | | |
| Essex | 1 | Niagara | 5 | Schoharie | 9 | | |

PREVAILING WAGE RATES – STATE AND FEDERALLY FUNDED PUBLIC WORKS CONTRACTS:

Work is subject to the prevailing wage rate provisions of New York State Labor Law. See "Prevailing Wage Rates - Public Works and Building Services Contracts" in Appendix B, OGS General Specifications. Any federal or State determination of a violation of any public works law or regulation, or labor law or regulation, or any OSHA violation deemed "serious or willful" may be grounds for a determination of Vendor non-responsibility and loss of contract.

Any provisions of NYS Labor Law that are in conflict with mandatory Federal-Aid construction contract compliance requirements are superseded. Any provisions of NYS Labor Law that are not in conflict with mandatory Federal-Aid construction contract compliance requirements or the Davis-Bacon Act but are more restrictive shall apply.

For access to the Department of Labor (DOL) Prevailing Wage Schedule, use the following link:

<http://wpp.labor.state.ny.us/wpp/showFindProject.do?method=showIt&id=723460>

Enter **2012002586**, the applicable Prevailing Wage Rate Schedule for this project, in the PRC# field and press the "Enter" key or click on "Submit". Next, click on "Wage Schedule" beneath the header. This schedule may require several minutes to open.

For Prevailing Wage Updates, use the following DOL link:

<http://wpp.labor.state.ny.us/wpp/publicViewPWChanges.do?method=showIt>

Links to schedule updates appear in the table at the bottom of the web page.

IMPORTANT NOTE: The above PRC number MUST be noted on all purchase orders issued for purchases from this contract.

The Federal Wage Rate Charts are located on the web at <http://www.wdol.gov/dba.aspx> .

Referring to the following tables, enter the applicable WD# in the "Select DBA by number" field on the web page and click "Search".

| COUNTY | WD # |
|-------------|------|
| Albany | NY2 |
| Allegany | NY47 |
| Cattaraugus | NY8 |
| Cayuga | NY36 |
| Chautauqua | NY8 |
| Chemung | NY5 |
| Clinton | NY6 |
| Cortland | NY42 |
| Erie | NY8 |

| COUNTY | WD # |
|------------|------|
| Essex | NY6 |
| Franklin | NY35 |
| Fulton | NY35 |
| Genesee | NY29 |
| Hamilton | NY46 |
| Jefferson | NY9 |
| Lewis | NY9 |
| Montgomery | NY2 |
| Niagara | NY11 |

| COUNTY | WD # |
|--------------|------|
| Oneida | NY14 |
| Onondaga | NY16 |
| Otsego | NY37 |
| Rensselaer | NY2 |
| Saratoga | NY2 |
| Schenectady | NY2 |
| Schuyler | NY5 |
| Seneca | NY40 |
| St. Lawrence | NY9 |

| COUNTY | WD # |
|------------|------|
| Steuben | NY18 |
| Sullivan | NY7 |
| Tompkins | NY24 |
| Washington | NY2 |
| Wayne | NY44 |
| Yates | NY33 |

WORKER NOTIFICATION – A9052; S6240

This provision is an addition to the existing prevailing wage rate law, Labor Law §220, paragraph a of subdivision 3-a. It requires contractors and subcontractors to provide written notice to all laborers, workers or mechanics of the prevailing wage rate for their particular job classification on each pay stub*. It also requires contractors and subcontractors to post a notice at the beginning of the performance of every public work contract on each job site that includes the telephone number and address for the Department of Labor and a statement informing laborers, workers or mechanics of their right to contact the Department of Labor if he/she is not receiving the proper prevailing rate of wages and/or supplements for his/her particular job classification. The required notification will be provided with each wage schedule, may be downloaded from www.labor.state.ny.us or made available upon request by contacting the Bureau of Public Work at 518-457-5589.

* In the event that the required information will not fit on the pay stub, an accompanying sheet or attachment of the information will suffice.

OSHA 10-Hour Construction Safety and Health Course - S1537-A

This provision is an addition to the existing prevailing wage rate law, Labor Law §220, section 220-h. It requires that on all public work contracts of at least \$250,000, all laborers, workers, and mechanics working on site be certified as having successfully completed the OSHA 10-hour construction safety and health course. It further requires that the advertised bids and contracts for every public work contract of at least \$250,000 contain a provision of the requirement AND only applies to workers on a public work project that are required under Article 8 to receive the prevailing wage.

Further information may be found at: www.labor.state.ny.us/workerprotection/publicwork/PWContents.shtm.

MATERIAL DESCRIPTIONS:

The following are the material descriptions of Superpave HMA items that may be included in this contract:

| | |
|--------------|--|
| 402.017902 | Truing & Leveling F9, 70 Series Compaction |
| 402.018902 | Truing & Leveling F9, 80 Series Compaction |
| 402.058902 | Shim Course F9 |
| 402.096102 | 9.5 F1, 60 Series Compaction |
| 402.096202 | 9.5 F2, 60 Series Compaction |
| 402.096302 | 9.5 F3, 60 Series Compaction |
| 402.097102 | 9.5 F1, 70 Series Compaction |
| 402.097202 | 9.5 F2, 70 Series Compaction |
| 402.097302 | 9.5 F3, 70 Series Compaction |
| 402.098302 | 9.5 F3, 80 Series Compaction |
| 402.098902 | 9.5 F9, Shoulder Course, 80 Series Compaction |
| 402.126102 | 12.5 F1, 60 Series Compaction |
| 402.126202 | 12.5 F2, 60 Series Compaction |
| 402.126302 | 12.5 F3, 60 Series Compaction |
| 402.127102 | 12.5 F1, 70 Series Compaction |
| 402.127202 | 12.5 F2, 70 Series Compaction |
| 402.127302 | 12.5 F3, 70 Series Compaction |
| 402.128902 | 12.5 F9, Shoulder Course, 80 Series Compaction |
| 402.196902 | 19 F9, 60 Series Compaction |
| 402.197902 | 19 F9, 70 Series Compaction |
| 402.256902 | 25 F9, 60 Series Compaction |
| 402.257902 | 25 F9, 70 Series Compaction |
| 402.06810118 | 6.3 F1, Superthin HMA, 80 Series Compaction |
| 402.06820118 | 6.3 F2, Superthin HMA, 80 Series Compaction |
| 402.06830118 | 6.3 F3, Superthin HMA, 80 Series Compaction |

PRE-PAVING CONFERENCE

The vendor shall schedule a Pre-Paving Conference with the affected Resident Engineer within one month after the award of the Contract and at least two weeks prior to the start of paving. At this conference the vendor shall present Certificates of Insurance evidencing compliance with the additional insurance requirements, their proposed paving schedule, equipment, proposed tack coat application procedure and paving procedure, and Work Zone Traffic Control Plan to the State for approval. At least one week prior to the start of paving, the vendor shall coordinate the details of the paving with the Resident Engineer.

SUPERVISION:

The Department of Transportation shall provide supervision for the paving operation. The Resident Engineer shall designate a Paving Supervisor and that person shall be in responsible charge of the operation. The following portions of Section 105 - CONTROL OF WORK of the Standard Specifications shall apply to these projects: 105-01 STOPPING WORK, 105-08 COOPERATION BY THE CONTRACTOR, 105-15 CONTRACTOR'S RESPONSIBILITY FOR WORK.

WORK HOURS:

Work shall not be permitted on Sundays and Holidays. If the vendor desires to work overtime on other days, they must obtain dispensation from the NYS Labor Department. Night work is prohibited unless agreed to by the Contractor and NYS Department of Transportation.

RESTORATION OF DISTURBED AREAS:

During the course of the work the vendor shall take reasonable care not to disturb areas outside the existing pavement. Any areas disturbed by the vendor shall be returned to their original condition at no expense to the State. Any and all debris generated as part of the work shall be removed by the vendor upon completion of the project.

TACK COAT:

The vendor shall provide and apply bituminous tack coat to all existing hot mix asphalt pavement surfaces to be overlaid in this contract (and to all hot mix asphalt pavement surfaces included in this contract that will be overlaid by this contract). The tack coat shall meet the materials requirements in Section 407-2 of the Standard Specifications. The application of tack coat shall comply with Section 407-3 of the Standard Specifications. **Tack coat shall be paid under its own item in gallons.**

CONSTRUCTION DETAILS:

The construction details shall comply with the requirements specified in Subsections 401-3.01, 402-3 and 407-3 of the Standard Specifications. The Paving Supervisor shall have sole responsibility for determining compliance with the specifications. All orders given to the vendor regarding construction details shall be considered final. **The tack coat application rate shall be 0.03 to 0.06 gallons per square yard as approved by the Paving Supervisor.** The pavement thicknesses and lane and shoulder widths shall be as specified elsewhere in this Invitation for Bids.

ATTENTION

Special Note - Conditioning:

The vendor will not be responsible for the initial conditioning of the existing pavement and shoulder surfaces as described in Section 402-3.05 of the NYSDOT Standard Specifications. Patching, joint repair, crack filling and the initial surface cleaning will be done by NYSDOT forces prior to the VPP project. However, once the VPP overlay placement begins, the vendor is responsible for keeping the pavement and shoulders clean until the overlay operations are completed, as per Section 633-3.01 of the NYSDOT Standard Specifications.

Special Note - Shoulder Edge

The outside edge of shoulder on any course of hot mix asphalt for all projects in this Invitation for Bids shall consist of a one on three tapered section from finished grade to the original surface constructed using a device attached to the screed. Hand work should be minimized. The top of the tapered section shall begin at the dimension indicated in the Project Dimensions table listed elsewhere in this Invitation for Bids. That is, the tapered section will be an additional width of material outside of the paved shoulder width specified in the Project Dimensions table.

CONTRACT BONDS:

The Contractor shall provide the State with a Labor and Materials Bond from a Surety Company listed on the U.S. Department of the Treasury listing of Approved Sureties (Treasury Department Circular 570) and licensed to do business in New York State, and with a minimum rating by A.M. Best of (A-) in the "best's Key Rating Guide". Treasury Department Circular 570 can be found on the U.S. Department of the Treasury website at www.fms.treas.gov/c570/index.html . **The Contractor shall procure and deliver the bond to the State at the Pre-Paving meeting** and shall maintain it at its own expense and without expense to the State during the Contract and until three months after the OGS contract ending date. If the contract is extended, the Labor and Materials Bond shall be extended until three months after the new contract ending date. The Surety Company shall append a statement of its financial condition and a copy of the resolution authorizing the execution of Bonds by the officers of the Company to the bond.

Labor and Material Bond. The Contractor shall provide a bond in the form prescribed by the Commissioner of the New York State Department of Transportation (NYSDOT), shown in the NYSDOT Standard Specification for Design and Construction, Sub-Section 103-08 Sample Form of Labor and Material Bond, with sufficient sureties, approved by said Commissioner, guaranteeing prompt payment of monies due all persons supplying the Contractor with labor and materials employed and used in carrying out the contract, which bond shall inure to the benefit of the persons supplying such labor and materials. The amount of the Labor and Material Bond shall be 100% of the amount of the total contract bid price.

103-08 SAMPLE FORM OF LABOR AND MATERIAL BOND.

KNOW ALL PERSONS BY THESE PRESENTS, that

(Name of Contractor)

(Address)

(hereinafter called the "Principal") and the

_____ a corporation created and existing under the laws of the State of _____ having its principal office in the City of _____ (hereinafter called the "Surety"), are held and firmly bound unto the People of the State of New York (hereinafter called the "State") by and through its Department of Transportation (hereinafter called the "Department"), in the full and just sum of [Total Contract Bid Price or the "A Portion" of Total Contract Bid Price Dollars (\$.....)] good and lawful money of the United States of America, for payment of which said sum of money, well and truly to be made and done, the said Principal binds itself, its heirs, executors and administrators, successors and assigns, and the said Surety binds itself, its successors and assigns jointly and severally, firmly by these presents:

WHEREAS, said Principal has entered into a certain written contract, on the ____ day of _____, 20____ with the Department of Transportation, 50 Wolf Road, Albany, New York 12232.

(Project Description)

In the county/counties of which constitutes Contract No. NOW, THEREFORE, the condition of this obligation is such, that if the said Principal shall promptly pay all monies due to all persons furnishing labor or materials to it or its SubContractors in the prosecution of the work provided for in said contract, then this obligation shall be void, otherwise to remain in full force and effect; Provided, however, that the Comptroller of the State of New York having required the said Principal to furnish this bond in order to comply with the provisions of Section 137 of the State Finance Law, all rights and remedies on this bond shall inure solely to such persons and shall be determined in accordance with the provisions, conditions and limitations of said Section to the same extent as if they were copied at length herein; and Further, provided, that the place of trial of any action on this bond shall be in the county in which the said contract was to be performed, or if said contract was to be performed in more than one county then in any such county, and not elsewhere.

IN TESTIMONY WHEREOF, the said Principal has hereunto set his/her (their, its) hand and the said Surety has caused this instrument to be signed by its authorized officer, the day and year above written.

Signed and delivered ____ day of _____ 20____ in the presence of

(Company)

By _____) Principal
(Signature)

(Title)

(Company)

By _____) Surety
(Signature)

(Title of Authorized Officer)

(The Surety Company shall append a single copy of a statement of its financial condition and a copy of the resolution authorizing the execution of Bonds by officers of the Company to the bond(s).

(Acknowledgment of principal, unless it be a corporation)

STATE OF NEW YORK ss. :

COUNTY OF _____

On this ____ day of _____ 20 ____, before me personally came _____ to me known and known to me to be the person described in and who executed the foregoing instrument and acknowledged that he/she executed the same.

Notary Public

(Acknowledgment of principal, if a corporation)

STATE OF NEW YORK ss. :

COUNTY _____

On this ____ day of _____ 20 ____, before me personally came _____ to me known and known to me to be the person, who being by me duly sworn, did depose and say that he/she resides in _____ that he/she is the _____ of the _____ the corporation described in and which executed the foregoing instrument; and that he/she signed his/her name thereto by order of the Board of Directors of said Corporation.

Notary Public

(Acknowledgment of Surety Company)

STATE OF NEW YORK ss. :

COUNTY OF _____

On this ____ day of _____ 20 ____, before me personally came _____ to me known and known to me to be the person, who being by me duly sworn, did depose and say that he/she resides in _____ that he/she is the _____ of the _____ the corporation described in the foregoing instrument; and that he/she signed his/her name thereto by order of the Board of Directors of said Corporation.

Notary Public

State Of New York Office of the Attorney General

I hereby approve the foregoing contract and bond as to form and manner of execution.

WORK ZONE TRAFFIC CONTROL

The vendor shall be responsible for Work Zone Traffic Control. Traffic shall be controlled in accordance with Sections 619-1 through 619-3 of the Standard Specifications, the Manual of Uniform Traffic Control Devices (MUTCD), and as described herein. The vendor shall submit a Work Zone Traffic Control Plan for approval to the Resident Engineer at the Pre-Paving Conference. For two-way roadways, Figures TAST-C1R, TAST-C2R, TAST-C3R, TAST-C4R, TAST-C5R, TAST-C7R, TAST-C1UL, TAST-C2UL, TAST-C3UL, TAST-C4U, TAST-C7UL, TAST-C1UH, TAST-C2UH, TAST-C3UH, and TAST-C7UH included in this document may be used as a basis for development of a Work Zone Traffic Control Plan. For one-way roadways, Figures TAST-C5UL, TAST-C6UL, TAST-C8UL, TAST-C5UH, TAST-C6UH, and TAST-C8UH may be used as a basis for development of a Work Zone Traffic Control Plan. For one-way Freeways or Expressways, Figures TAST-E1, TAST-E2, TAST-E3, TAST-E4, TAST-E5, TAST-E6, and TAST-E7 may be used as a basis for development of a Work Zone Traffic Control Plan.

All necessary flaggers for Work Zone Traffic Control shall be provided by the vendor. For two-way roadways, a minimum of three flaggers shall be provided while the paving operation is underway. One shall be stationed at each end of the operation and one shall be stationed with the paver. For one-way roadways, a minimum of two flaggers shall be provided while the paving operation is underway. One shall be stationed at the beginning of the operation and one shall be stationed with the paver. The vendor shall station flaggers such that communication is maintained between the flaggers. Hand signals, radios, or some other means of communication may be used subject to the approval of the Resident Engineer.

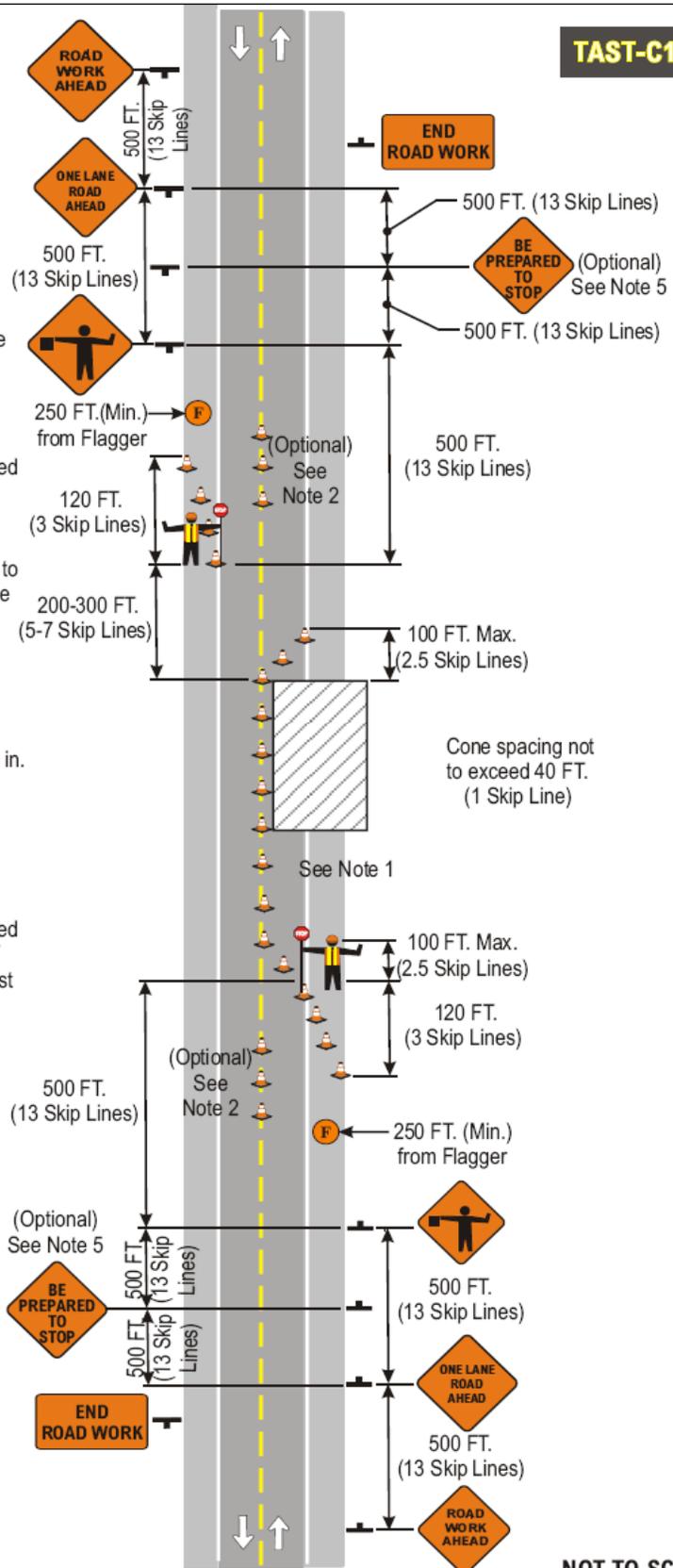
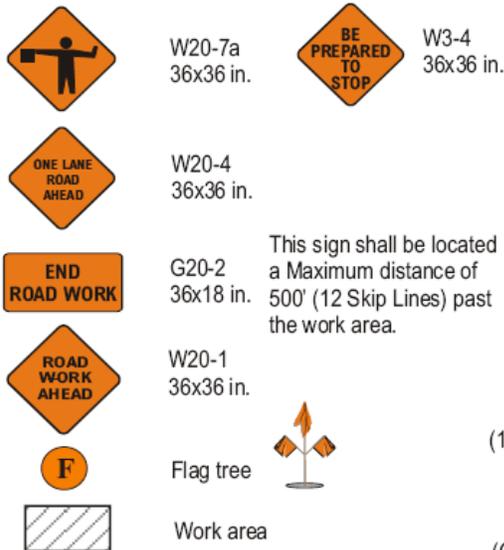
All costs for Work Zone Traffic Control including flagging, temporary pavement marking, and construction signs are included in the price per ton. No separate payment shall be made.

RURAL HIGHWAY

TAST-C1R

Notes:

- Barrier vehicles are not required for flagging operations. A buffer space may be provided where traffic conditions allow.
- Centerline cones are optional and may be eliminated where space constraints exist. If cones are used, place them 100 FT. (Min.) From flagger.
- State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.
- Flagger Symbol Sign (W20-7a) and "ONE LANE ROAD AHEAD" Sign (W20-4) shall be removed, covered or turned away from road users when flagging operations are not occurring.
- Should the traffic queue up prior to the advance warning signs the "BE PREPARED TO STOP" sign can be added to the sign series at the location shown or the entire advance warning sign series shall be moved to a location prior to the queued traffic.



**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
DAYTIME
LANE CLOSURE WITH FLAGGERS
ON
RURAL
TWO-LANE CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C1R

NOT TO SCALE

TAST-C3R

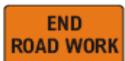
RURAL HIGHWAY

Notes:

- Barrier vehicles are not required for flagging operations. A buffer space may be provided where traffic conditions allow.
- Centerline cones are optional and may be eliminated where space constraints exist. If cones are used, place them 100 FT. (Min.) From flagger.
- State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.
- "BE PREPARED TO STOP" Sign (W3-4) and "ONE LANE ROAD AHEAD" Sign (W20-4) shall be removed, covered or turned away from road users when flagging operations are not occurring.



W20-4
36x36 in.



G20-2
36x18 in.

This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.



W20-1
36x36 in.



W3-4
36x36 in.

Automated Flagger Device with Stop/Slow Panels and Signs



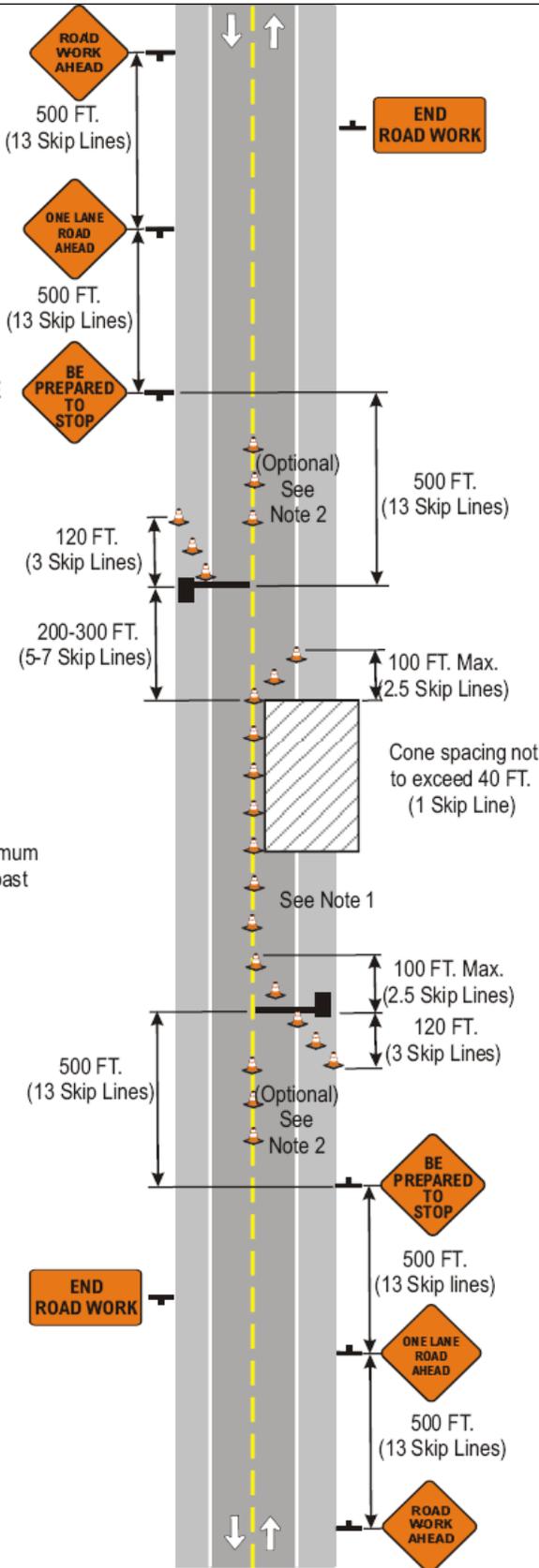
Work Area

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
DAYTIME
LANE CLOSURE WITH TWO
AUTOMATED FLAGGER DEVICES
(STOP/SLOW PANELS)
ON
RURAL
TWO-LANE CONVENTIONAL HIGHWAY**

FEBRUARY 2009

TAST-C3R



NOT TO SCALE

RURAL HIGHWAY

TAST-C4R

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 300-500 FT. In advance of initial warning sign.

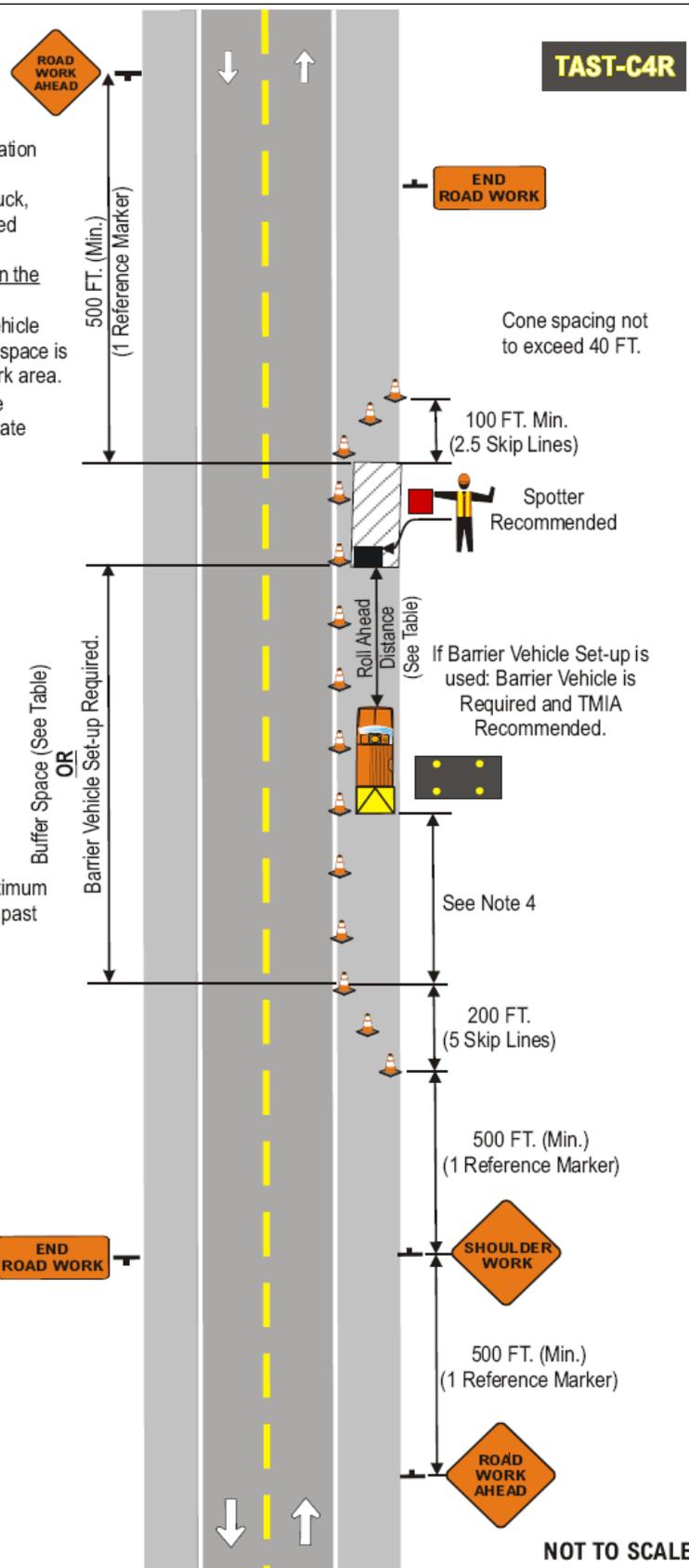
| Speed Limit (Mph) | Buffer Space | Roll Ahead Distance |
|-------------------|-----------------------|---------------------|
| 30 | 200' (5 Skip Lines) | 40' (1 Skip Line) |
| 35 | 250' (~6 Skip Lines) | 40' (1 Skip Line) |
| 40 | 305' (~8 Skip Lines) | 40' (1 Skip Line) |
| 45 | 360' (9 Skip Lines) | 80' (2 Skip Lines) |
| 50 | 425' (~11 Skip Lines) | 80' (2 Skip Lines) |
| 55 | 495' (~13 Skip Lines) | 80' (2 Skip Lines) |

END ROAD WORK G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

ROAD WORK AHEAD W20-1 36x36 in. **SHOULDER WORK** W21-5 36x36 in.

 Work Area
 Arrow Panel (Caution Mode)
 Barrier Vehicle with TMIA

NYSDOT
WORK ZONE TRAFFIC CONTROL
SHORT-TERM STATIONARY
 OPERATION INVOLVING
SHOULDER CLOSURE
 (PAVED SHOULDER 8 FT. OR WIDER)
 ON
RURAL
TWO-LANE CONVENTIONAL HIGHWAY
 FEBRUARY 2009 TAST-C4R



NOT TO SCALE

RURAL HIGHWAY

TAST-C5R

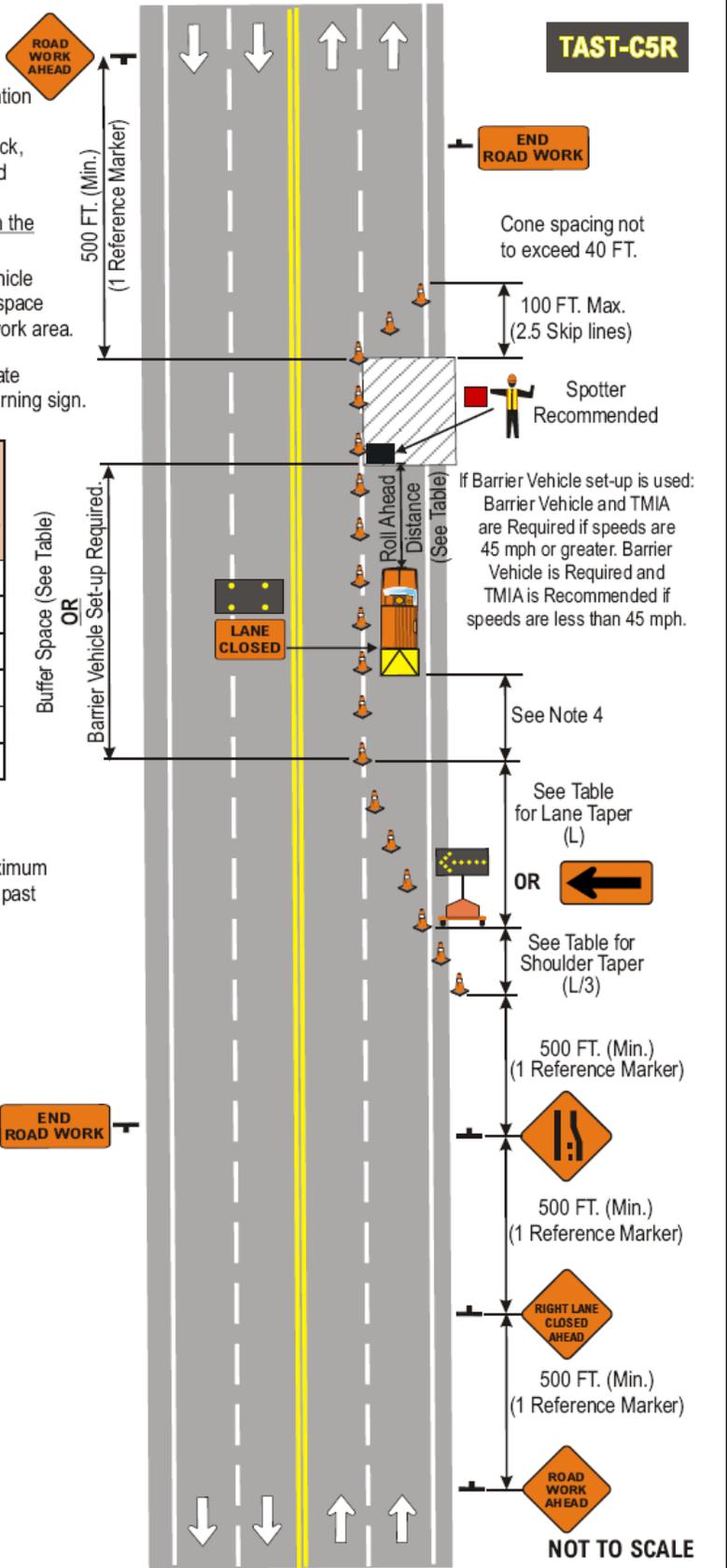
Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 300-500 FT. In advance of initial warning sign.

| Speed Limit (Mph) | Buffer Space | Taper Lengths based on Lane Shift | | | Shoulder Taper based on 4'-6' Shift | Roll Ahead Distance |
|-------------------|--------------|-----------------------------------|------|------|-------------------------------------|---------------------|
| | | 10' | 11' | 12' | | |
| 30 | 200' | 150' | 165' | 180' | 20'-30' | 40' |
| 35 | 250' | 205' | 225' | 245' | 25'-40' | 40' |
| 40 | 305' | 270' | 300' | 320' | 35'-50' | 40' |
| 45 | 360' | 450' | 495' | 540' | 60'-90' | 80' |
| 50 | 425' | 500' | 550' | 600' | 70'-100' | 80' |
| 55 | 495' | 550' | 605' | 660' | 75'-110' | 80' |

W1-6L 48x24 in.
 G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.
 W4-2R 36x36 in. NYW8-33 48x24 in.
 W20-1 36x36 in. W20-5R 36x36 in.
 Arrow Panel (Caution Mode)
 Barrier Vehicle with TMIA Work Area

NYSDOT
WORK ZONE TRAFFIC CONTROL
SHORT-TERM STATIONARY
 OPERATION INVOLVING
RIGHT LANE CLOSURE
 (PAVED SHOULDER LESS THAN 8 FT.)
 ON
RURAL
MULTI-LANE CONVENTIONAL HIGHWAY
 FEBRUARY 2009 TAST-C5R



RURAL HIGHWAY

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
3. The Barrier Vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area. The buffer space is is from the end of the lane taper to the beginning of the work area.
6. If using BOTH a Barrier vehicle and a Buffer Space, first place the Barrier Vehicle at the required roll-ahead distance from the work area, and then provide as much Buffer Space as practicable.
7. State Law signs (NYR9-11 or NYR9-12) are optional if the the anticipated work duration is less that 4 hours. If used, State Law sign is to be placed 300 FT. In advance of the initial warning sign.

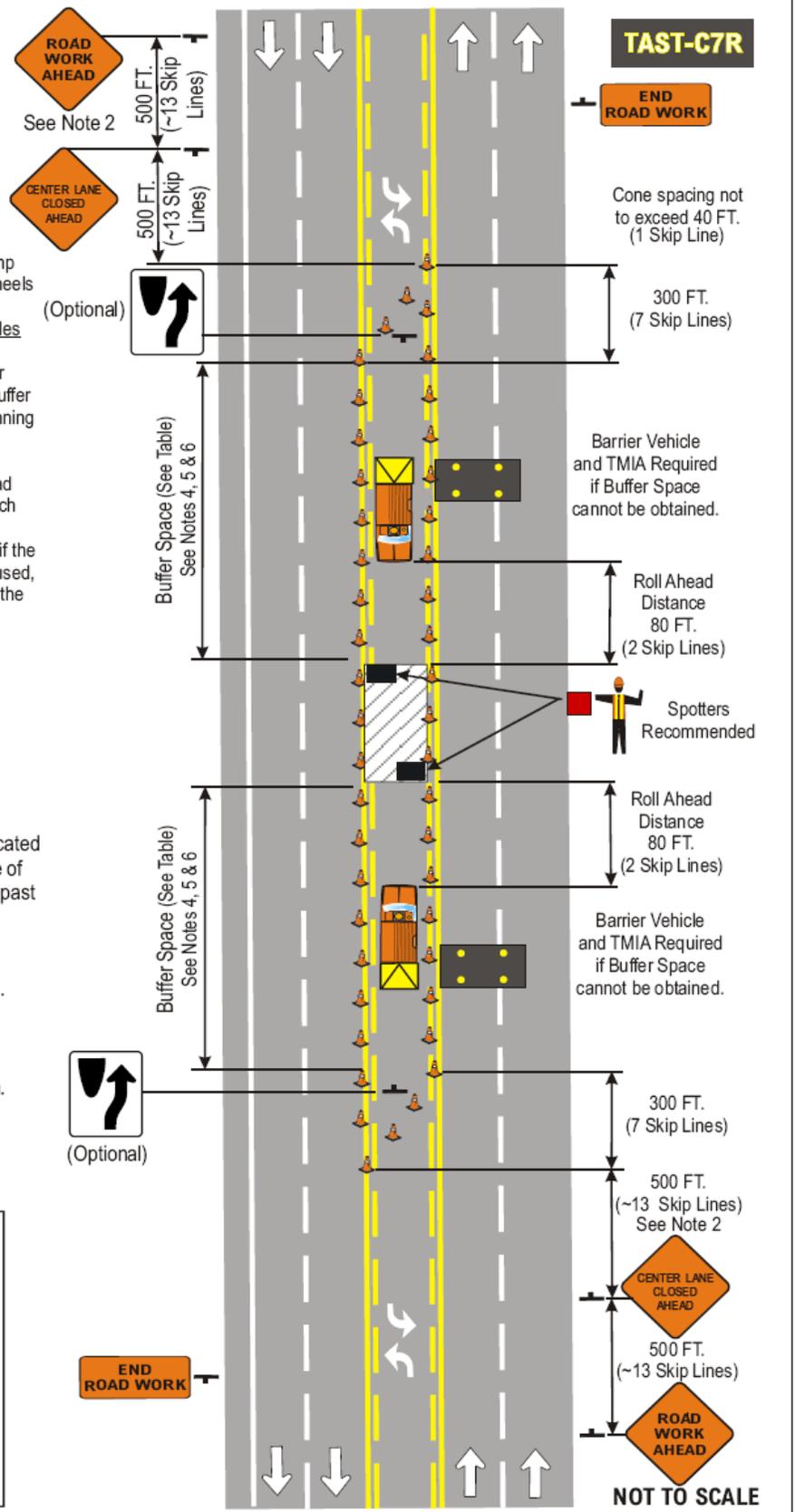
| Speed Limit (Mph) | Buffer Space |
|-------------------|-----------------------|
| 45 | 360' (9 Skip Lines) |
| 50 | 425' (~11 Skip Lines) |
| 55 | 495' (~13 Skip Lines) |

- G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.
- W20-1 36x36 in.
- W20-5 36x36 in.
- Work Area
- R4-7 24x30 in.
- Arrow Panel (Caution Mode)
- Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
TWO-WAY LEFT TURN LANE CLOSURE
ON
RURAL
CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C7R



TAST-C7R

END ROAD WORK

Cone spacing not to exceed 40 FT. (1 Skip Line)

300 FT. (7 Skip Lines)

Barrier Vehicle and TMIA Required if Buffer Space cannot be obtained.

Roll Ahead Distance 80 FT. (2 Skip Lines)

Spotters Recommended

Roll Ahead Distance 80 FT. (2 Skip Lines)

Barrier Vehicle and TMIA Required if Buffer Space cannot be obtained.

300 FT. (7 Skip Lines)

500 FT. (~13 Skip Lines) See Note 2

CENTER LANE CLOSED AHEAD

500 FT. (~13 Skip Lines)

ROAD WORK AHEAD

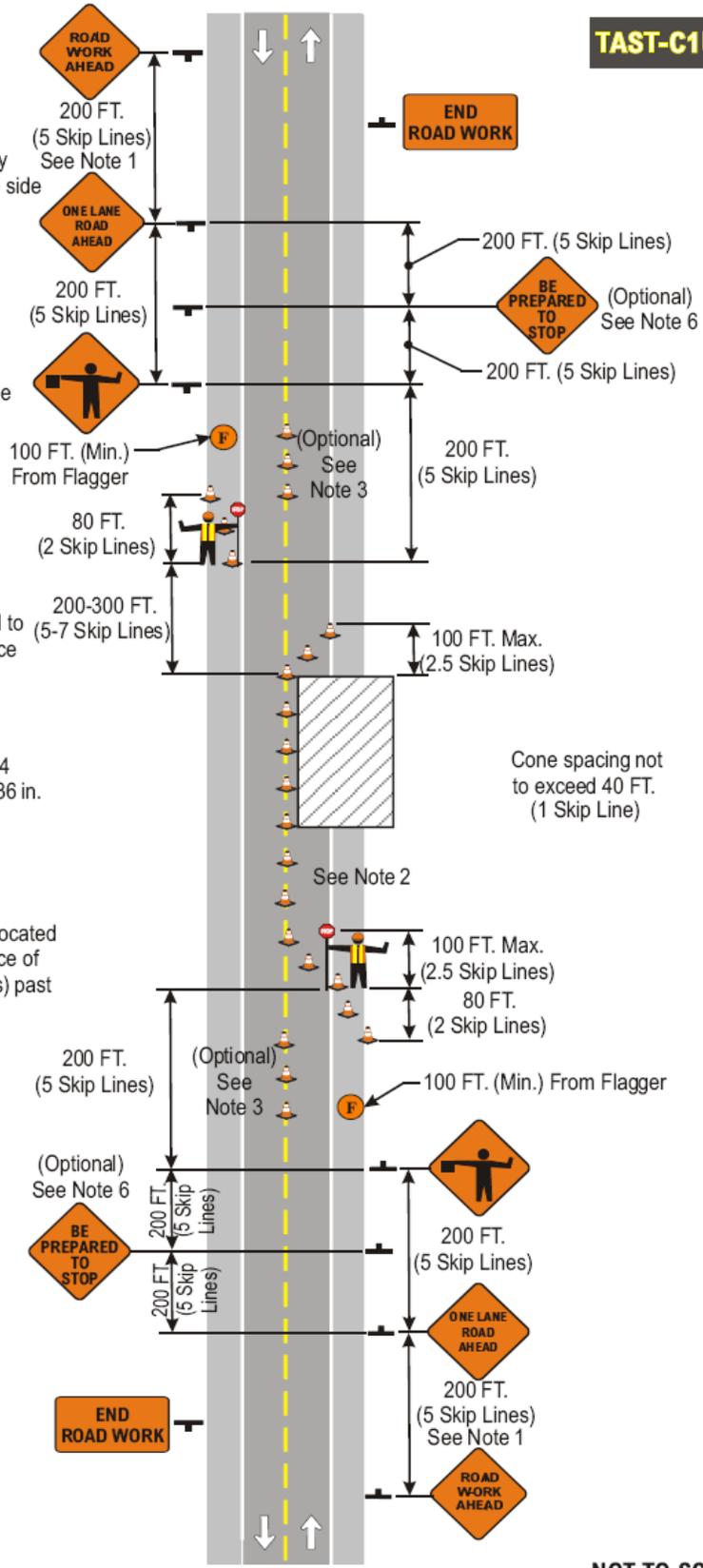
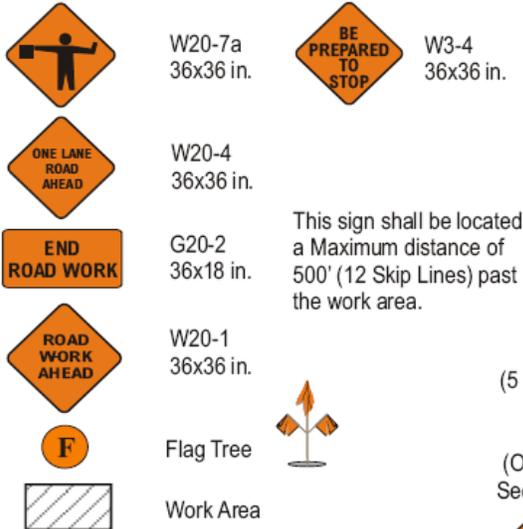
NOT TO SCALE

TAST-C1UL

**LOW SPEED URBAN HIGHWAY
(30-40 mph)**

Notes:

1. In urban conditions, advance warning sign spacings may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
2. Barrier vehicles are not required for flagging operations. A buffer space may be provided where traffic conditions allow.
3. Centerline cones are optional and may be eliminated where space constraints exist. If cones are used, place them 100 FT. (Min.) From flagger.
4. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.
5. Flagger Symbol Sign (W20-7a) and "ONE LANE ROAD AHEAD" Sign (W20-4) shall be removed, covered or turned away from road users when flagging operations are not occurring.
6. Should the traffic queue up prior to the advance warning signs the "BE PREPARED TO STOP" sign can be added to the sign series at the location shown or the entire advance warning sign series shall be moved to a location prior to the queued traffic.



**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
DAYTIME
LANE CLOSURE WITH FLAGGERS
ON
LOW SPEED URBAN (30-40 mph)
TWO-LANE CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C1UL

NOT TO SCALE

**LOW SPEED URBAN HIGHWAY
(30-40 mph)**

TAST-C2UL

Notes:

1. In urban conditions, advance warning sign spacings may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
2. Barrier vehicles are not required for flagging operations. A buffer space may be provided where traffic conditions allow.
3. Centerline cones are optional and may be eliminated where space constraints exist. If cones are used, place them 100 FT. (Min.) from flagger.
4. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.
5. Flagger Symbol Sign (W20-7a) and "ONE LANE ROAD AHEAD" Sign (W20-4) shall be removed, covered or turned away from road users when flagging operations are not occurring.
6. Should the traffic queue up prior to the advance warning signs the "BE PREPARED TO STOP" sign can be added to the sign series at the location shown or the entire advance warning sign series shall be moved to a location prior to the queued traffic.



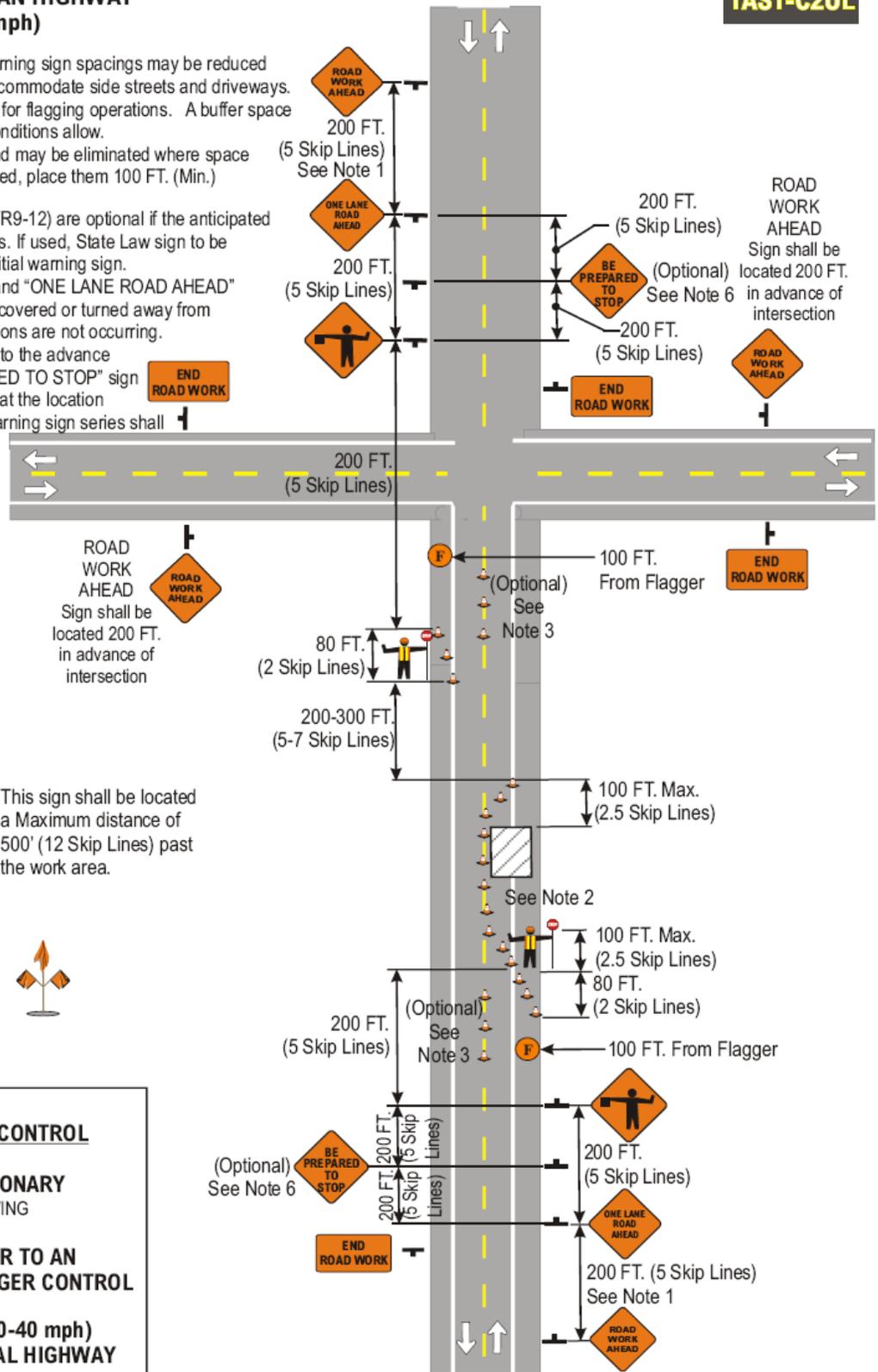
This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.



**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
DAYTIME
LANE CLOSURE PRIOR TO AN
INTERSECTION WITH FLAGGER CONTROL
ON
LOW SPEED URBAN (30-40 mph)
TWO-LANE CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C2UL



NOT TO SCALE

URBAN HIGHWAY

TAST-C4U

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced (100 FT. Min.) in order to accommodate side streets and driveways.
3. The barrier vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 300-500 FT. In advance of initial warning sign.

| Speed Limit (Mph) | Buffer Space | Roll Ahead Distance |
|-------------------|-----------------------|---------------------|
| 30 | 200' (5 Skip Lines) | 40' (1 Skip Line) |
| 35 | 250' (~6 Skip Lines) | 40' (1 Skip Line) |
| 40 | 305' (~8 Skip Lines) | 40' (1 Skip Line) |
| 45 | 360' (9 Skip Lines) | 80' (2 Skip Lines) |
| 50 | 425' (~11 Skip Lines) | 80' (2 Skip Lines) |
| 55 | 495' (~13 Skip Lines) | 80' (2 Skip Lines) |



G20-2
36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.



W20-1
36x36 in.



W21-5
36x36 in.



Work Area



Arrow Panel
(Caution Mode)



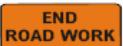
Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
SHOULDER CLOSURE
(PAVED SHOULDER 8 FT. OR WIDER)
ON
URBAN
TWO-LANE CONVENTIONAL HIGHWAY**

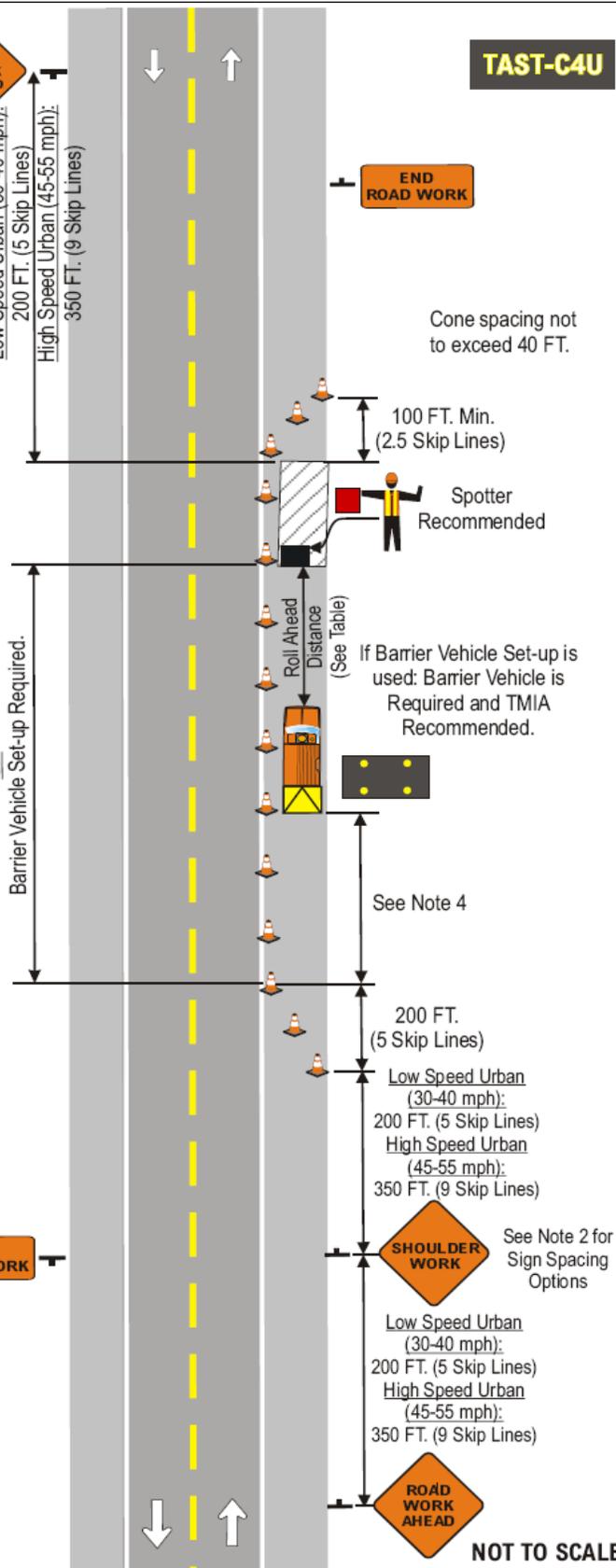
FEBRUARY 2009

TAST-C4U



Low Speed Urban (30-40 mph):
200 FT. (5 Skip Lines)
High Speed Urban (45-55 mph):
350 FT. (9 Skip Lines)

Buffer Space (See Table)
OR
Barrier Vehicle Set-up Required.



Cone spacing not to exceed 40 FT.

100 FT. Min.
(2.5 Skip Lines)

Spotter Recommended

If Barrier Vehicle Set-up is used: Barrier Vehicle is Required and TMIA Recommended.

See Note 4

200 FT.
(5 Skip Lines)

Low Speed Urban (30-40 mph):
200 FT. (5 Skip Lines)
High Speed Urban (45-55 mph):
350 FT. (9 Skip Lines)

SHOULDER WORK See Note 2 for Sign Spacing Options

Low Speed Urban (30-40 mph):
200 FT. (5 Skip Lines)
High Speed Urban (45-55 mph):
350 FT. (9 Skip Lines)

NOT TO SCALE

**LOW SPEED URBAN HIGHWAY
(30-40 mph)**

TAST-C5UL

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
3. The Barrier Vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign is to be placed 300 FT. in advance of the initial warning sign.

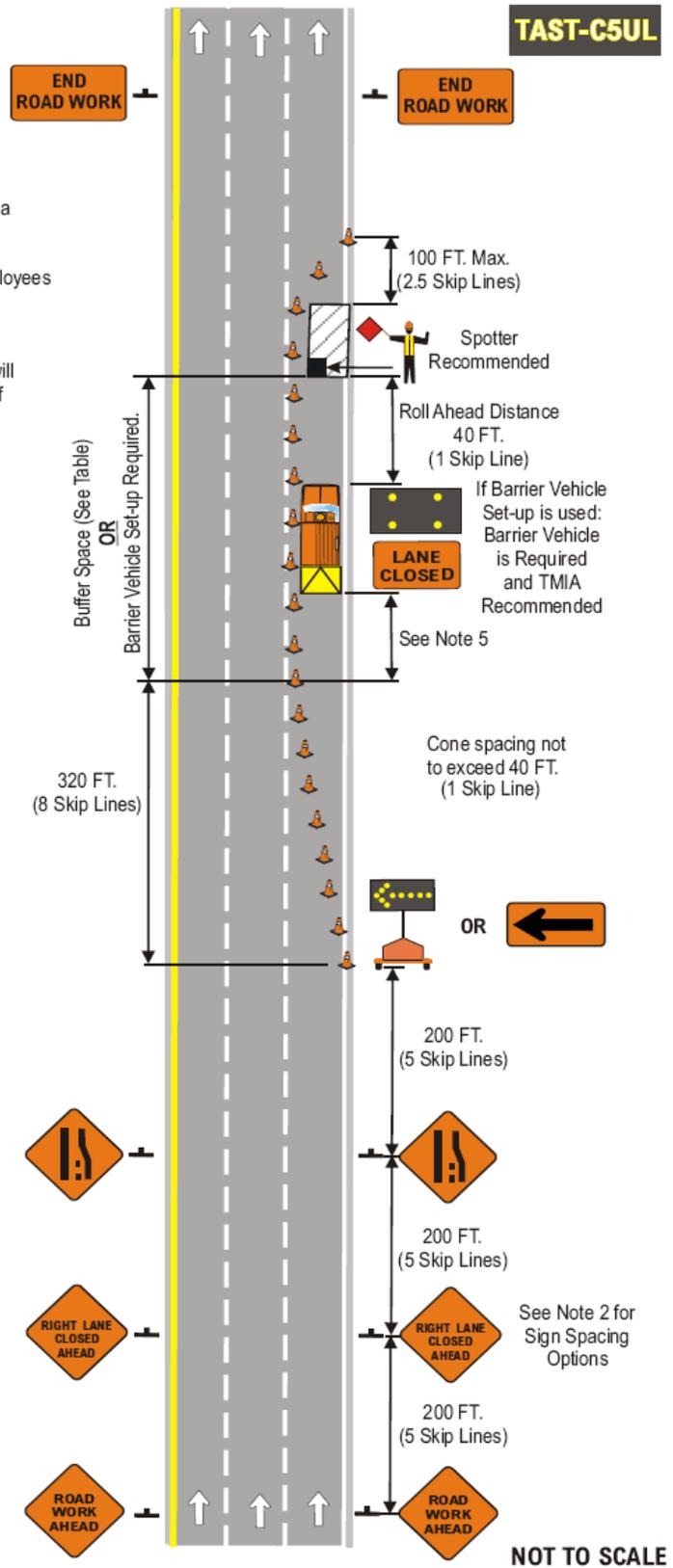
| Speed Limit (Mph) | Buffer Space |
|-------------------|----------------------|
| 30 | 200' (5 Skip Lines) |
| 35 | 250' (~7 Skip Lines) |
| 40 | 305' (~8 Skip Lines) |

| | | | | |
|--|----------------------------|---|--|---------------------------|
| | G20-2 36x18 in. | This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area. | | W4-2R 36x36 in. |
| | W20-1 36x36 in. | | | W20-5R 36x36 in. |
| | NYW8-33 48x24 in. | | | Barrier Vehicle with TMIA |
| | Arrow Panel (Caution Mode) | | | Work Area |
| | W1-6L 48x24 in. | | | |

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT LANE CLOSURE
(CURBED OR NO SHOULDER)
ON
LOW SPEED URBAN (30-40 mph)
ONE-WAY CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C5UL



**LOW SPEED URBAN HIGHWAY
(30-40 mph)**

END ROAD WORK

END ROAD WORK

TAST-C6UL

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
3. The Barrier Vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less that 4 hours. If used, State Law sign is to be placed 300 FT. In advance of the initial warning sign.

| Speed Limit (Mph) | Buffer Space |
|-------------------|----------------------|
| 30 | 200' (5 Skip Lines) |
| 35 | 250' (~7 Skip Lines) |
| 40 | 305' (~8 Skip Lines) |

END ROAD WORK G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

RIGHT TWO LANES CLOSED AHEAD W20-5aR 36x36 in. **LANE ENDS MERGE LEFT** W9-2L 36x36 in.

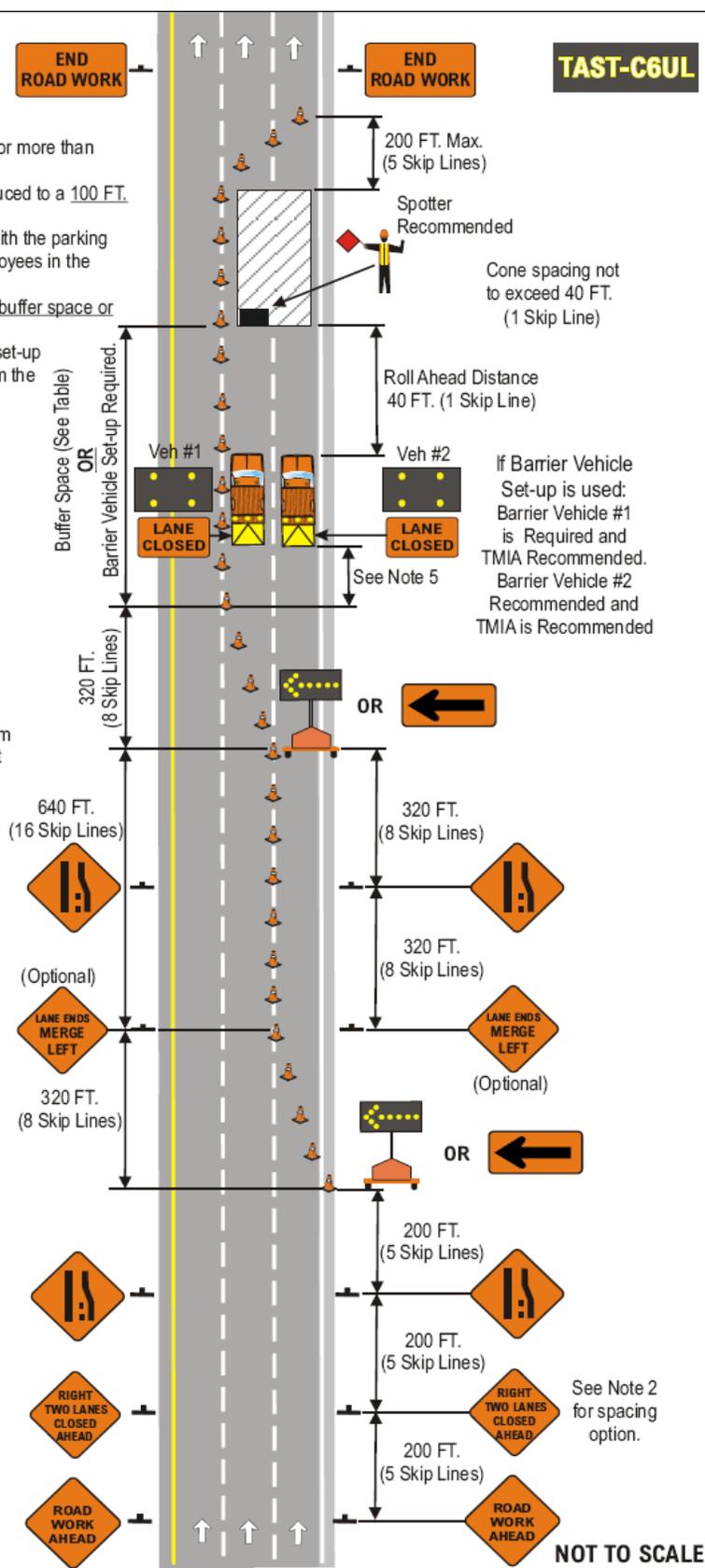
ROAD WORK AHEAD W20-1 36x36 in. **LANE ENDS MERGE LEFT** W4-2R 36x36 in.

LANE CLOSED NYW8-33 48x24 in. **←** W1-6L 48x24 in.

Work Area

Arrow Panel (Caution Mode)

Barrier Vehicle with TMIA



**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT TWO LANE CLOSURE
(CURBED OR NO SHOULDER)
ON
LOW SPEED URBAN (30-40 mph)
ONE-WAY CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C6UL

**LOW SPEED URBAN HIGHWAY
(30-40 mph)**

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
3. The Barrier Vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. If using BOTH a Barrier vehicle and a Buffer Space, first place the Barrier Vehicle at the required roll-ahead distance from the work area, and then provide as much Buffer Space as practicable.
7. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign is to be placed 300 FT. In advance of the initial warning sign.

| Speed Limit (Mph) | Buffer Space |
|-------------------|----------------------|
| 30 | 200' (5 Skip Lines) |
| 35 | 250' (~7 Skip Lines) |
| 40 | 305' (~8 Skip Lines) |

END ROAD WORK G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

ROAD WORK AHEAD W20-1 36x36 in. **CENTER LANE CLOSED AHEAD** W20-5 36x36 in.

Work Area **R4-7** 24x30 in.

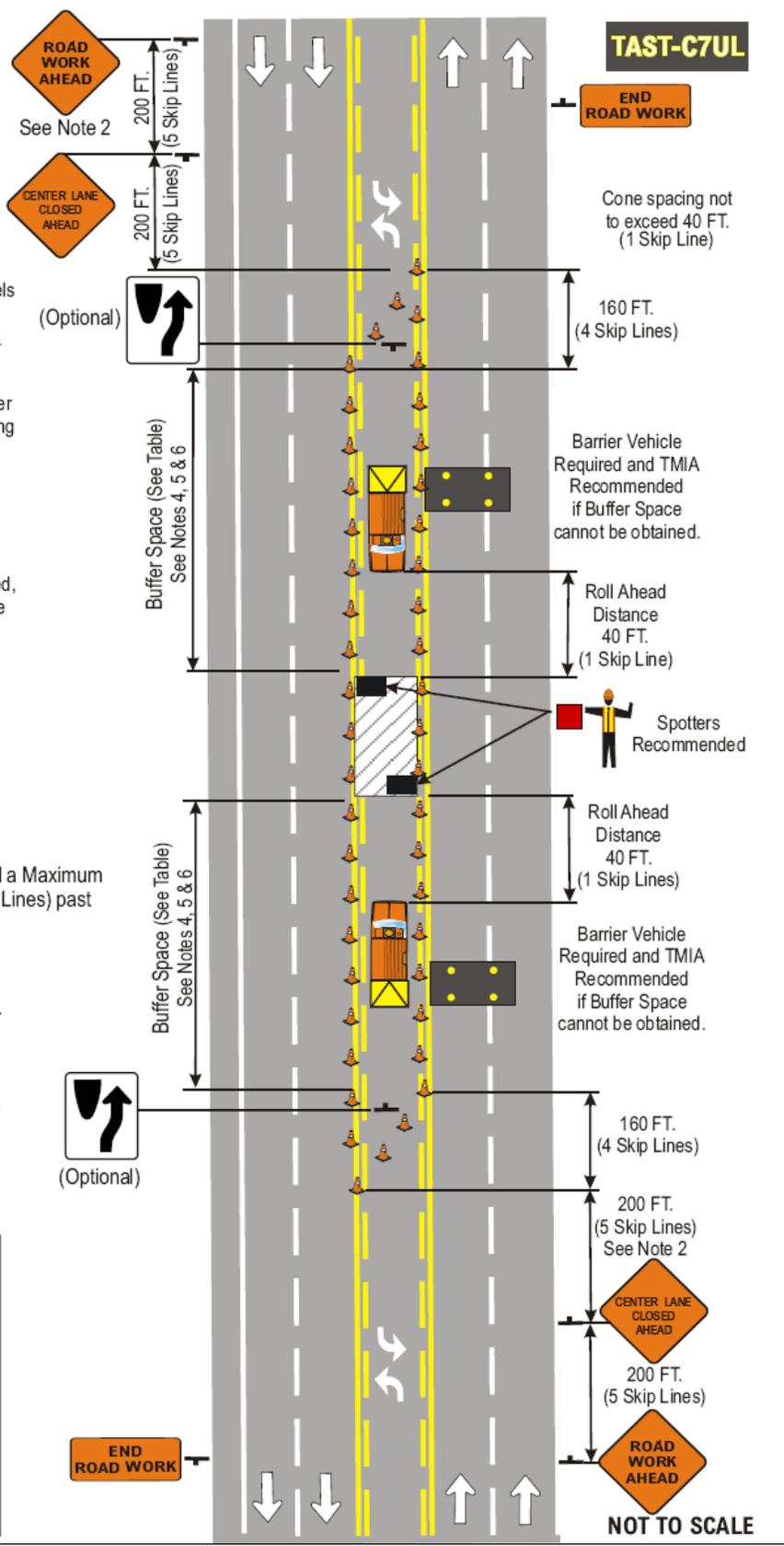
Arrow Panel (Caution Mode)

Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
TWO-WAY LEFT TURN LANE CLOSURE
ON
LOW SPEED URBAN (30-40 mph)
CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C7UL



**LOW SPEED URBAN HIGHWAY
(30-40 mph)**

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced (100 FT. Min.) in order to accommodate side streets and driveways.
3. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 300 FT in advance of initial warning sign.
7. Place "ROAD WORK AHEAD" signs on side road 100-200 FT. In advance of intersection.

| Speed Limit (Mph) | Buffer Space |
|-------------------|----------------------|
| 30 | 200' (5 Skip Lines) |
| 35 | 250' (~7 Skip Lines) |
| 40 | 305' (~8 Skip Lines) |

LANE CLOSED NYW8-33
36x18 in.



W4-2R
36x36 in.



W1-6L
48x24 in.



W20-1
36x36 in.



W20-5R
36x36 in.



G20-2
36x18 in.

This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.



Work Area



Arrow Panel
(Caution Mode)



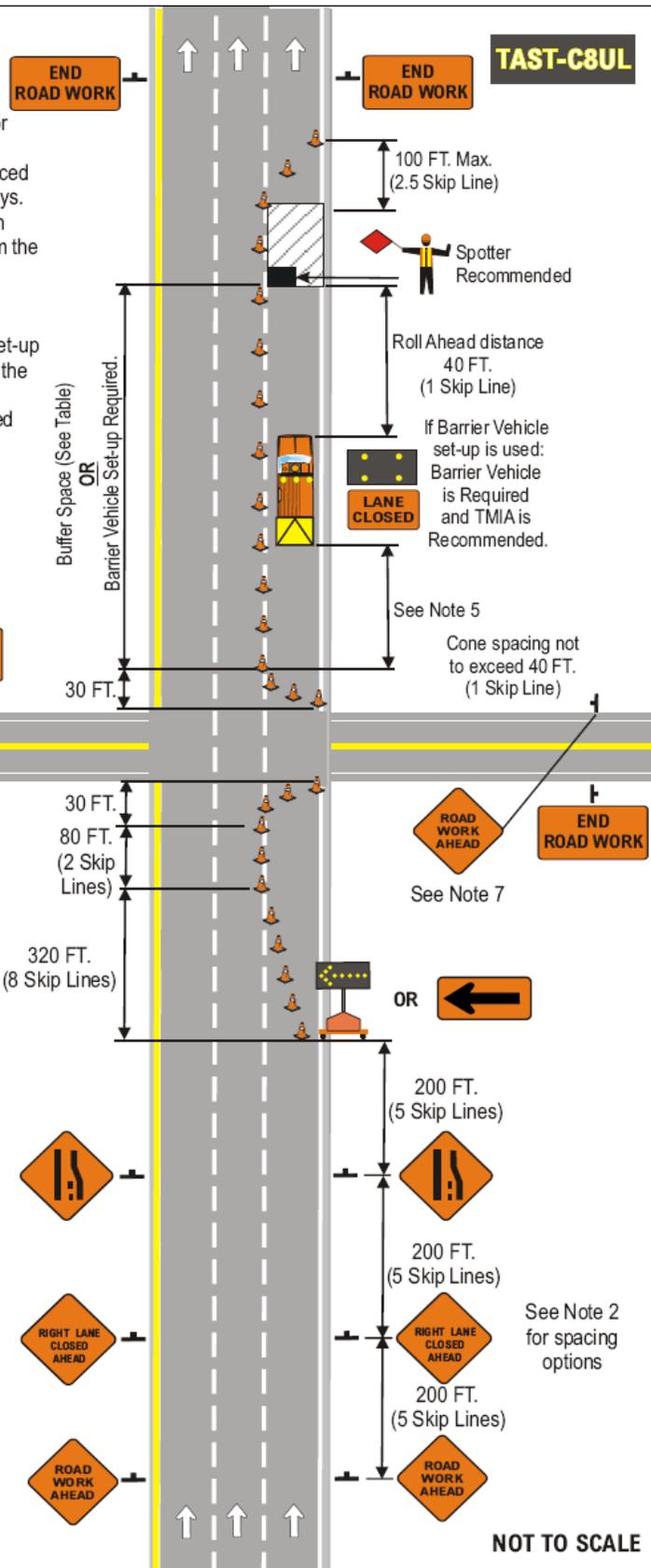
Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT LANE CLOSURE THRU INTERSECTION
(CURBED OR NO SHOULDER)
ON
LOW SPEED URBAN (30-40 mph)
ONE-WAY CONVENTIONAL HIGHWAY**

FEBRUARY 2009

TAST-C8UL



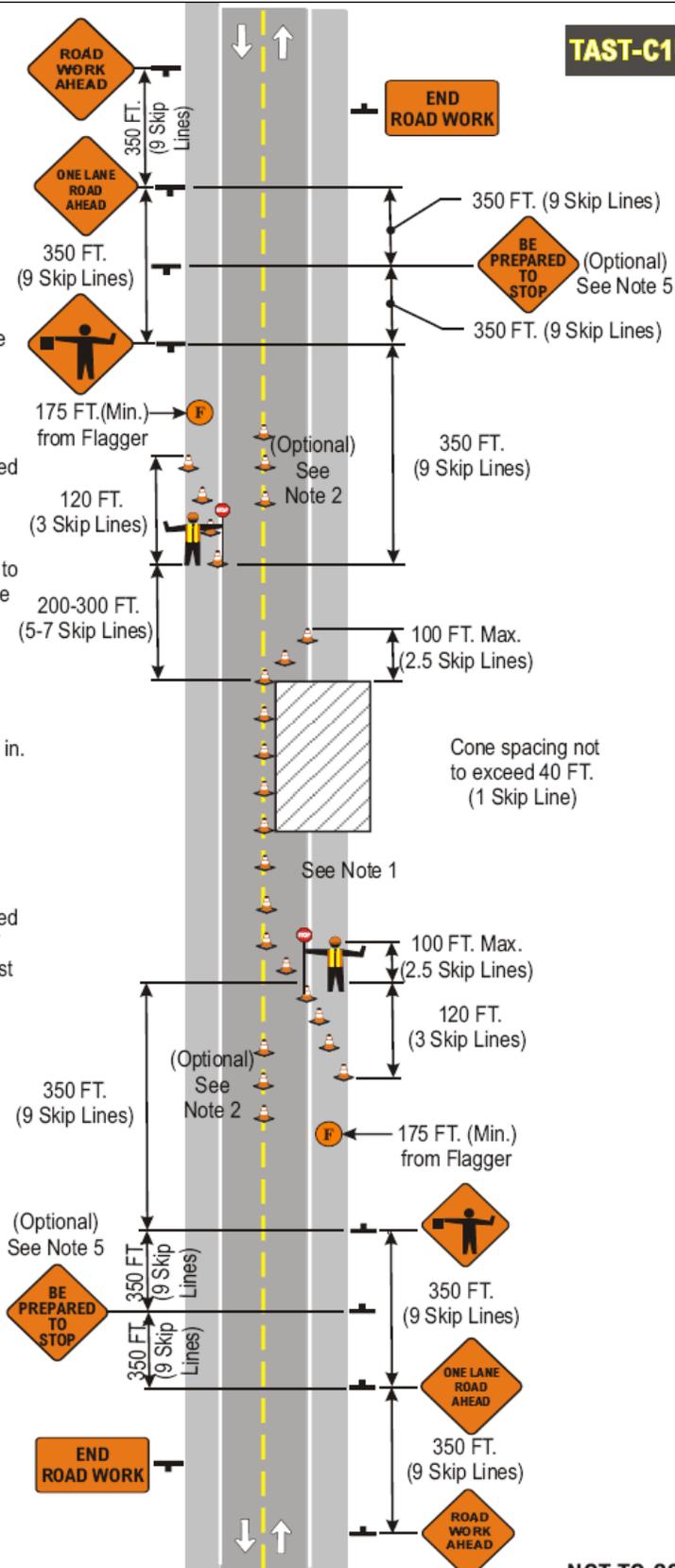
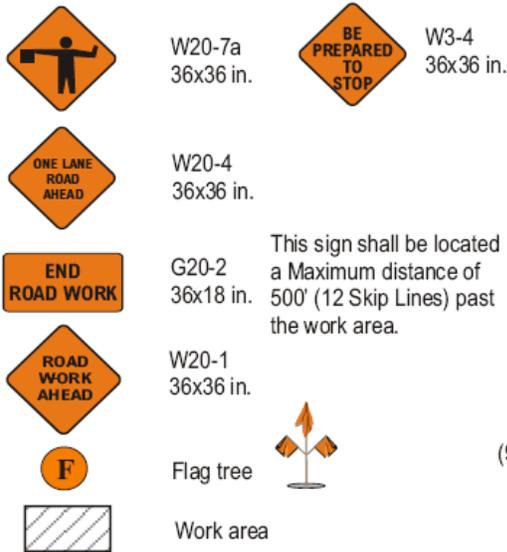
NOT TO SCALE

**HIGH SPEED URBAN HIGHWAY
(45-55 mph)**

TAST-C1UH

Notes:

- Barrier vehicles are not required for flagging operations. A buffer space may be provided where traffic conditions allow.
- Centerline cones are optional and may be eliminated where space constraints exist. If cones are used, place them 100 FT. (Min.) From flagger.
- State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.
- Flagger Symbol Sign (W20-7a) and "ONE LANE ROAD AHEAD" Sign (W20-4) shall be removed, covered or turned away from road users when flagging operations are not occurring.
- Should the traffic queue up prior to the advance warning signs the "BE PREPARED TO STOP" sign can be added to the sign series at the location shown or the entire advance warning sign series shall be moved to a location prior to the queued traffic.

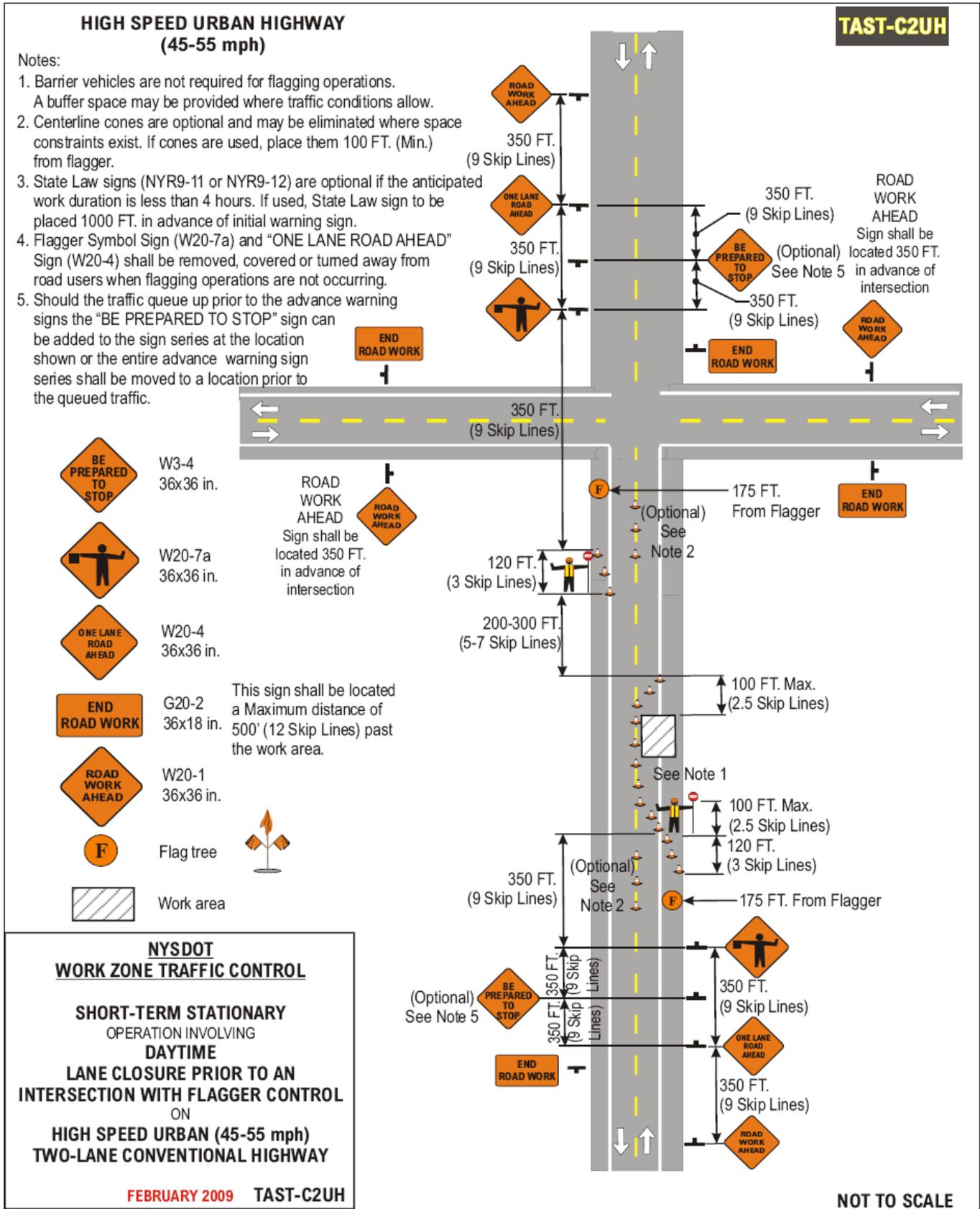


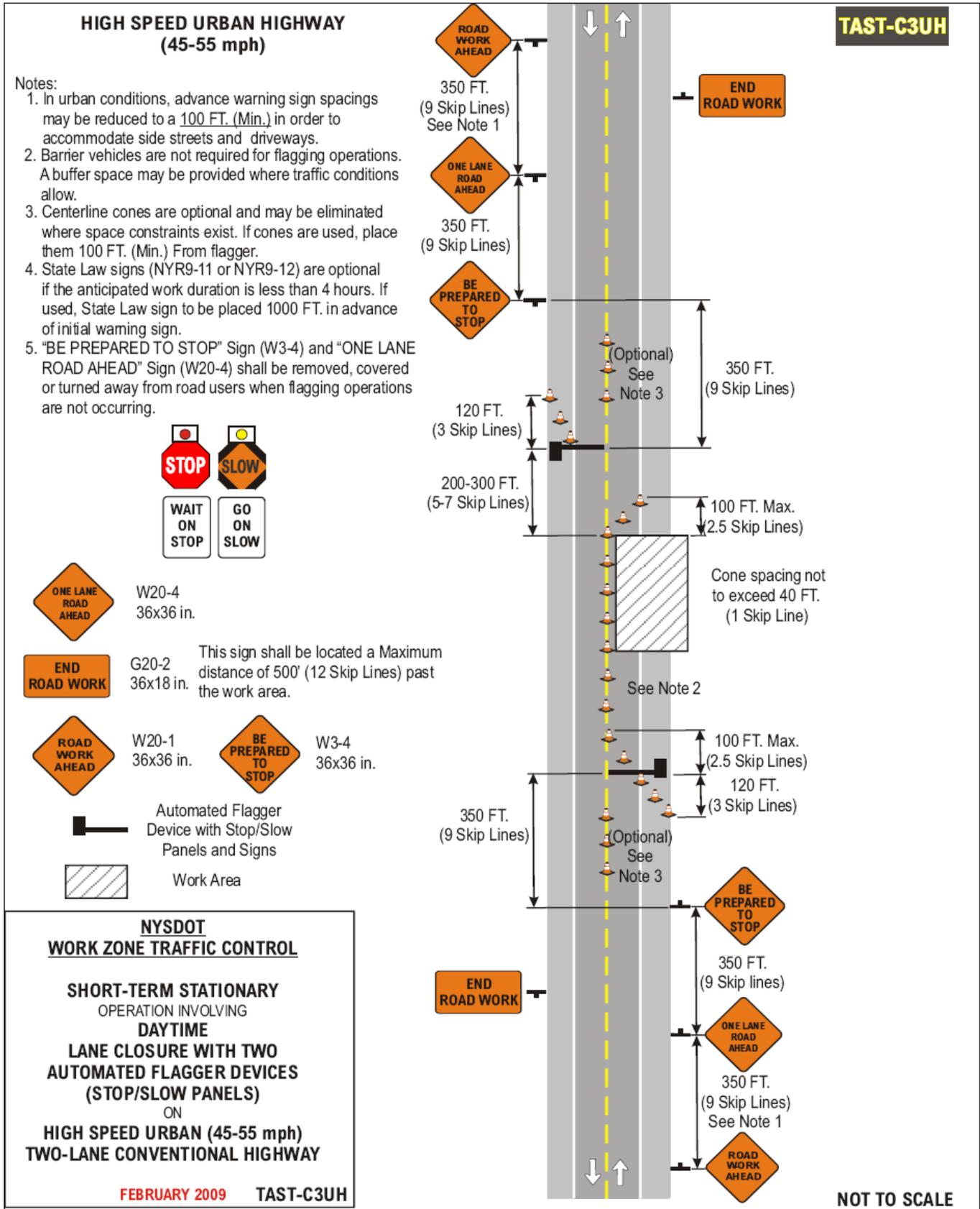
**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
DAYTIME
LANE CLOSURE WITH FLAGGERS
ON
HIGH SPEED URBAN (45-55 mph)
TWO-LANE CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C1UH

NOT TO SCALE





**HIGH SPEED URBAN HIGHWAY
(45-55 mph)**

TAST-C5UH

Notes:

- Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
- In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
- The Barrier Vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
- There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
- Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
- State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign is to be placed 300 FT. in advance of the initial warning sign.

| Speed Limit (Mph) | Buffer Space |
|-------------------|-----------------------|
| 45 | 360' (9 Skip Lines) |
| 50 | 425' (~11 Skip Lines) |
| 55 | 495' (~13 Skip Lines) |

END ROAD WORK G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

ROAD WORK AHEAD W20-1 36x36 in. **ROAD WORK AHEAD** W4-2R 36x36 in.

LANE CLOSED NYW8-33 48x24 in. **RIGHT LANE CLOSED AHEAD** W20-5R 36x36 in.

← W1-6L 48x24 in.

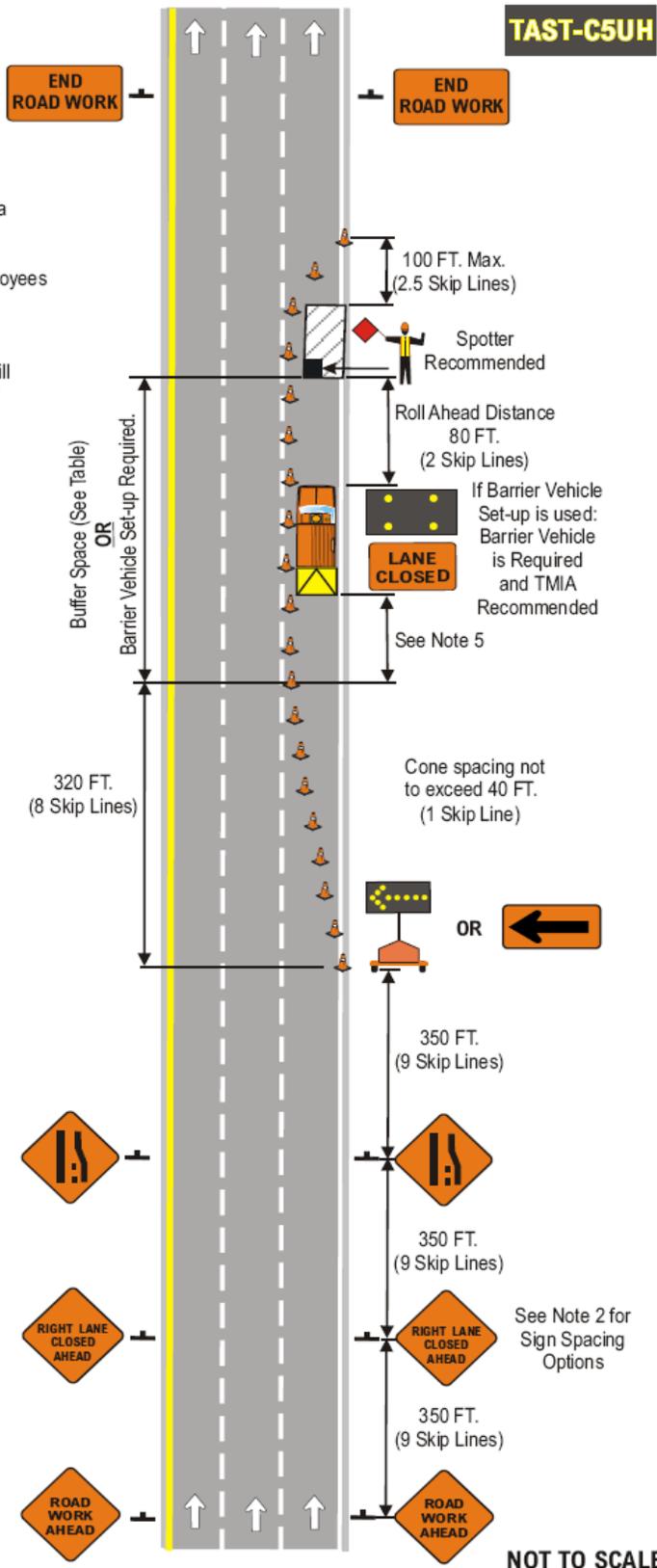
Arrow Panel (Caution Mode)

Barrier Vehicle with TMIA **Work Area**

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT LANE CLOSURE
(PAVED SHOULDER LESS THAN 8 FT.)
ON
HIGH SPEED URBAN (45-55 mph)
ONE-WAY CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C5UH



NOT TO SCALE

**HIGH SPEED URBAN HIGHWAY
(45-55 mph)**

TAST-C6UH

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
3. The Barrier Vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign is to be placed 300 FT. in advance of the initial warning sign.

| Speed Limit (Mph) | Buffer Space |
|-------------------|-----------------------|
| 45 | 360' (9 Skip Lines) |
| 50 | 425' (~11 Skip Lines) |
| 55 | 495' (~13 Skip Lines) |

END ROAD WORK G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

RIGHT TWO LANES CLOSED AHEAD W20-5aR 36x36 in. **LANE ENDS MERGE LEFT** W9-2L 36x36 in.

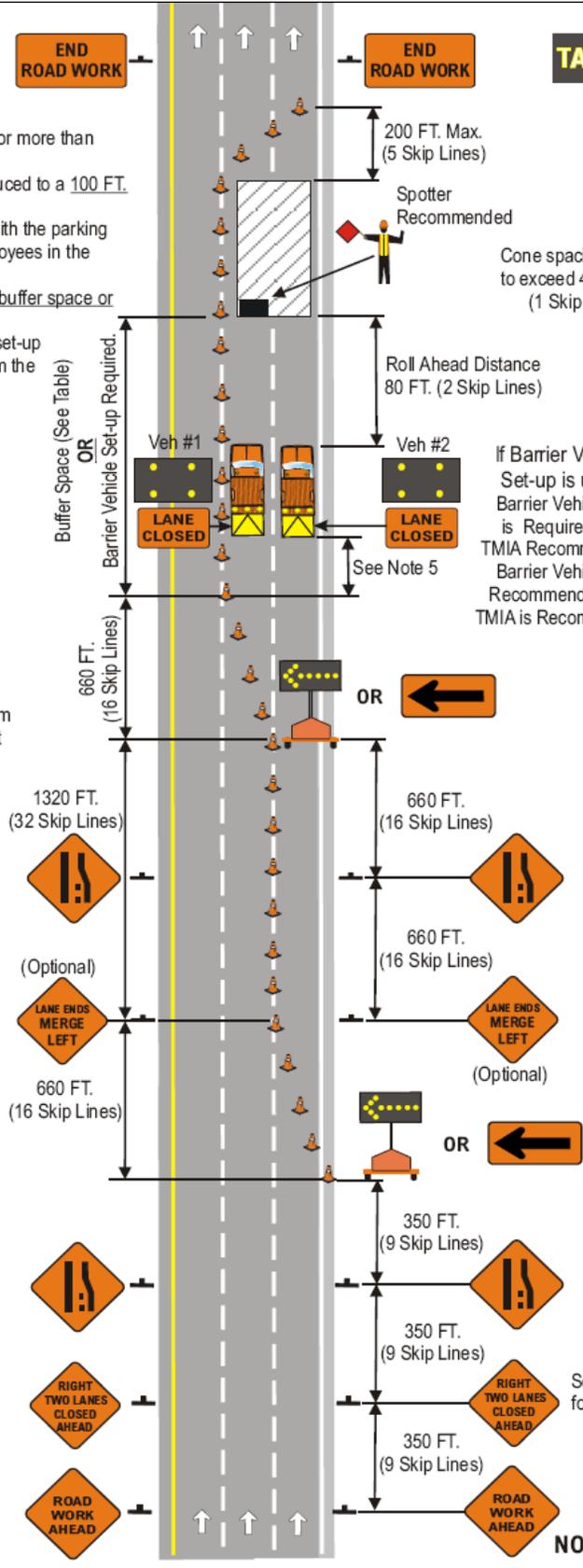
ROAD WORK AHEAD W20-1 36x36 in. **LANE ENDS MERGE LEFT** W4-2R 36x36 in.

LANE CLOSED NYW8-33 48x24 in. **LANE CLOSED** W1-6L 48x24 in.

Work Area

Arrow Panel (Caution Mode)

Barrier Vehicle with TMIA



Cone spacing not to exceed 40 FT. (1 Skip Line)

If Barrier Vehicle Set-up is used: Barrier Vehicle #1 is Required and TMIA Recommended. Barrier Vehicle #2 Recommended and TMIA is Recommended

See Note 2 for spacing option.

NOT TO SCALE

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT TWO LANE CLOSURE
(CURBED OR NO SHOULDER)
ON
HIGH SPEED URBAN (45-55 mph)
ONE-WAY CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C6UH

**HIGH SPEED URBAN HIGHWAY
(45-55 mph)**

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT.(Min.) in order to accommodate side streets and driveways.
3. The Barrier Vehicle shall be an unoccupied large dump truck, with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area. The buffer space is is from the end of the lane taper to the beginning of the work area.
6. If using BOTH a Barrier vehicle and a Buffer Space, first place the Barrier Vehicle at the required roll-ahead distance from the work area, and then provide as much Buffer Space as practicable.
7. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less that 4 hours. If used, State Law sign is to be placed 300 FT. In advance of the initial warning sign.

| Speed Limit (Mph) | Buffer Space |
|-------------------|-----------------------|
| 45 | 360' (9 Skip Lines) |
| 50 | 425' (~11 Skip Lines) |
| 55 | 495' (~13 Skip Lines) |

END ROAD WORK G20-2 36x18 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

ROAD WORK AHEAD W20-1 36x36 in. **CENTER LANE CLOSED AHEAD** W20-5 36x36 in.

Work Area **R4-7** 24x30 in.

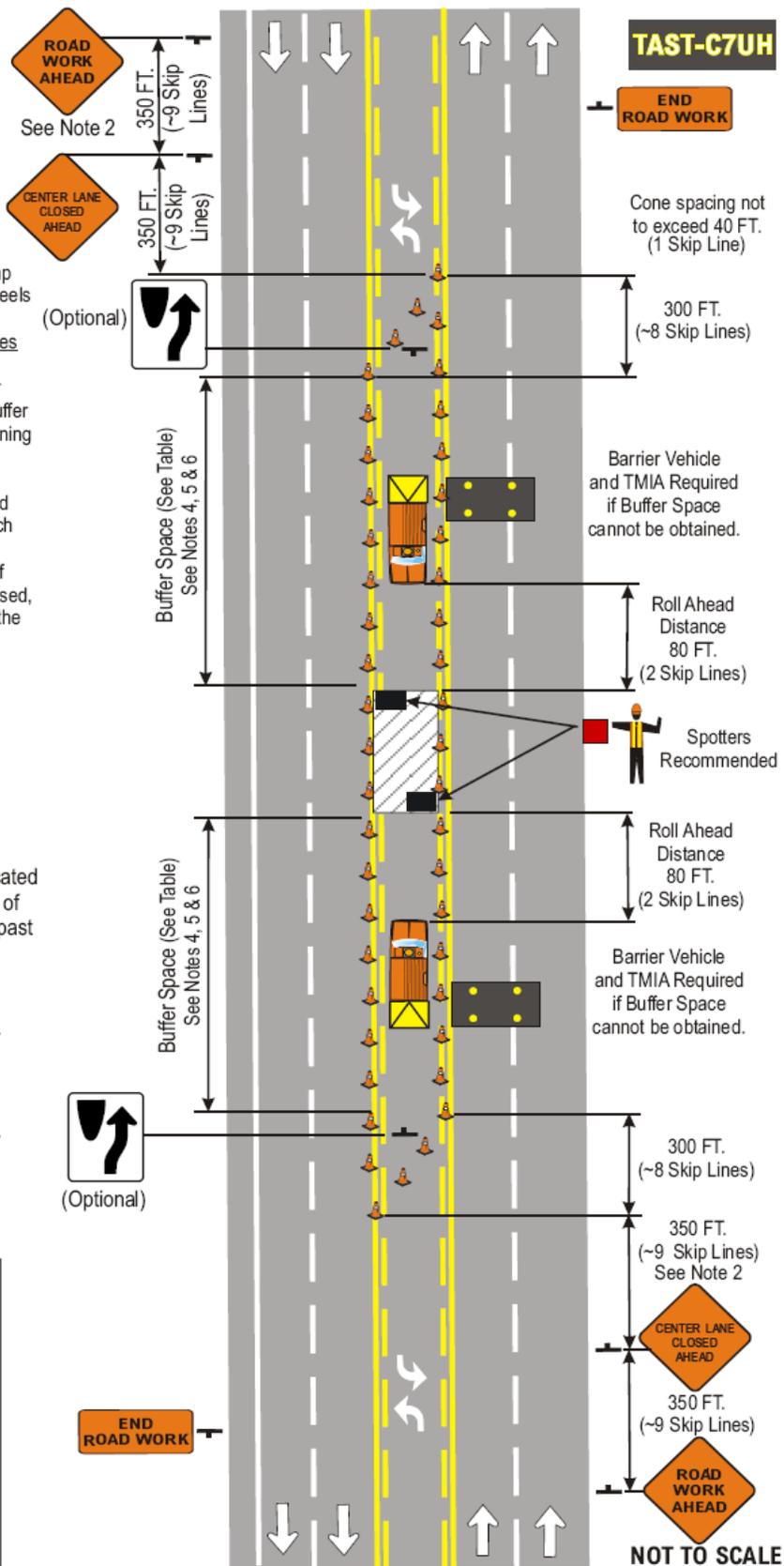
Arrow Panel (Caution Mode)

Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
TWO-WAY LEFT TURN LANE CLOSURE
ON
HIGH SPEED URBAN (45-55 mph)
CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C7UH



**HIGH SPEED URBAN HIGHWAY
(45-55 mph)**

TAST-C8UH

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. In urban conditions, advance warning sign spacing may be reduced to a 100 FT. (Min.) in order to accommodate side streets and driveways.
3. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
4. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
5. Any buffer distance provided in advance of the barrier vehicle set-up will add to the safety of the work area. The buffer space is from the end of the lane taper to the beginning of the work area.
6. State Law signs (NYR9-11 or NYR9-12) are optional if the anticipated work duration is less than 4 hours. If used, State Law sign to be placed 300 FT. in advance of initial warning sign.
7. Place "ROAD WORK AHEAD" signs on side road 100-200 FT. in advance of intersection.

| Speed Limit (Mph) | Buffer Space |
|-------------------|-----------------------|
| 45 | 360' (9 Skip Lines) |
| 50 | 425' (~11 Skip Lines) |
| 55 | 495' (~13 Skip Lines) |

LANE CLOSED NYW8-33
36x18 in.



W4-2R
36x36 in.



W1-6L
48x24 in.



W20-1
36x36 in.



W20-5R
36x36 in.



G20-2
36x18 in.

This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.



Work Area



Arrow Panel
(Caution Mode)

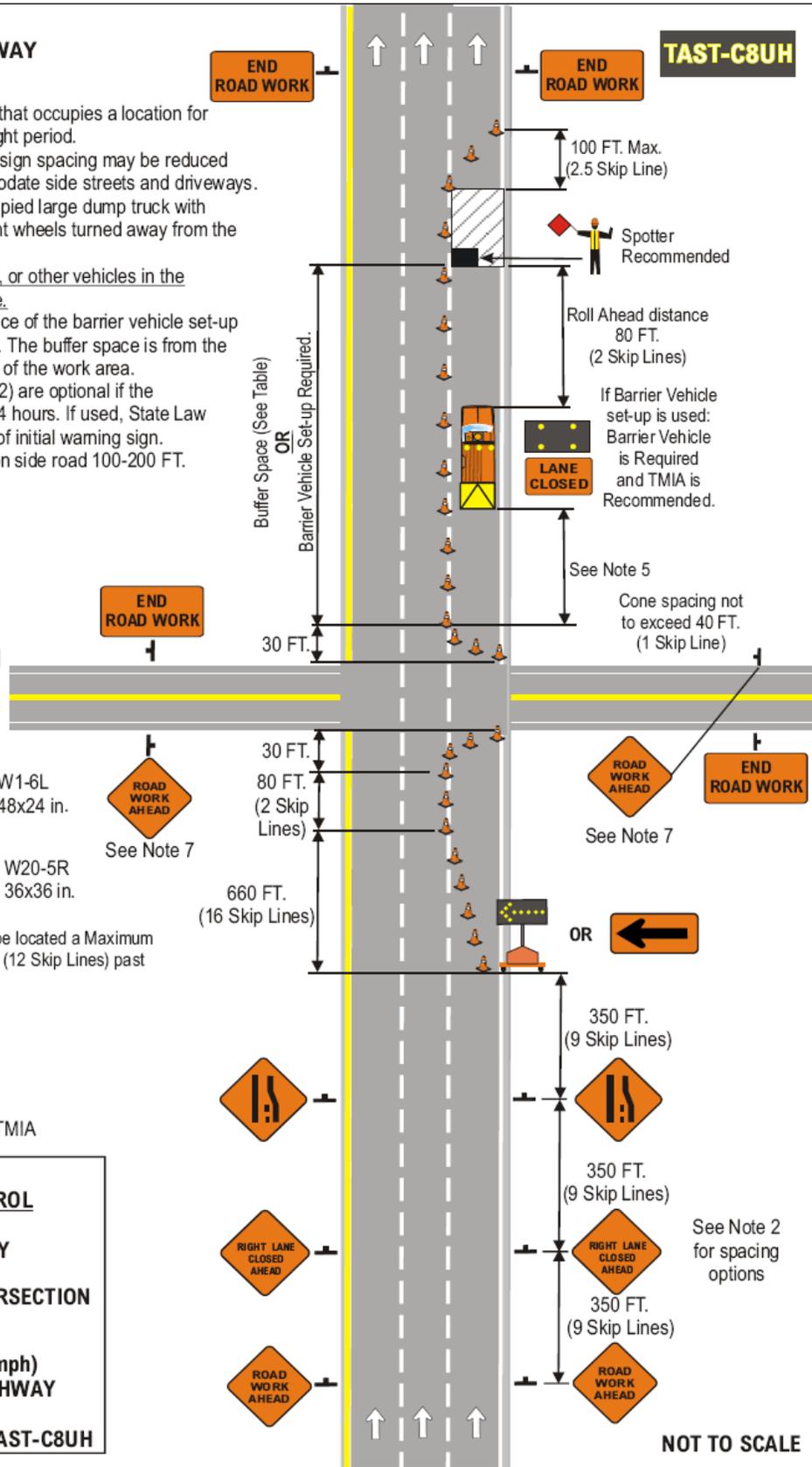


Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT LANE CLOSURE THRU INTERSECTION
(CURBED OR NO SHOULDER)
ON
HIGH SPEED URBAN (45-55 mph)
ONE-WAY CONVENTIONAL HIGHWAY**

FEBRUARY 2009 TAST-C8UH



NOT TO SCALE

FREEWAY OR EXPRESSWAY

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set left in low or reverse gear and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.

TAST-E1

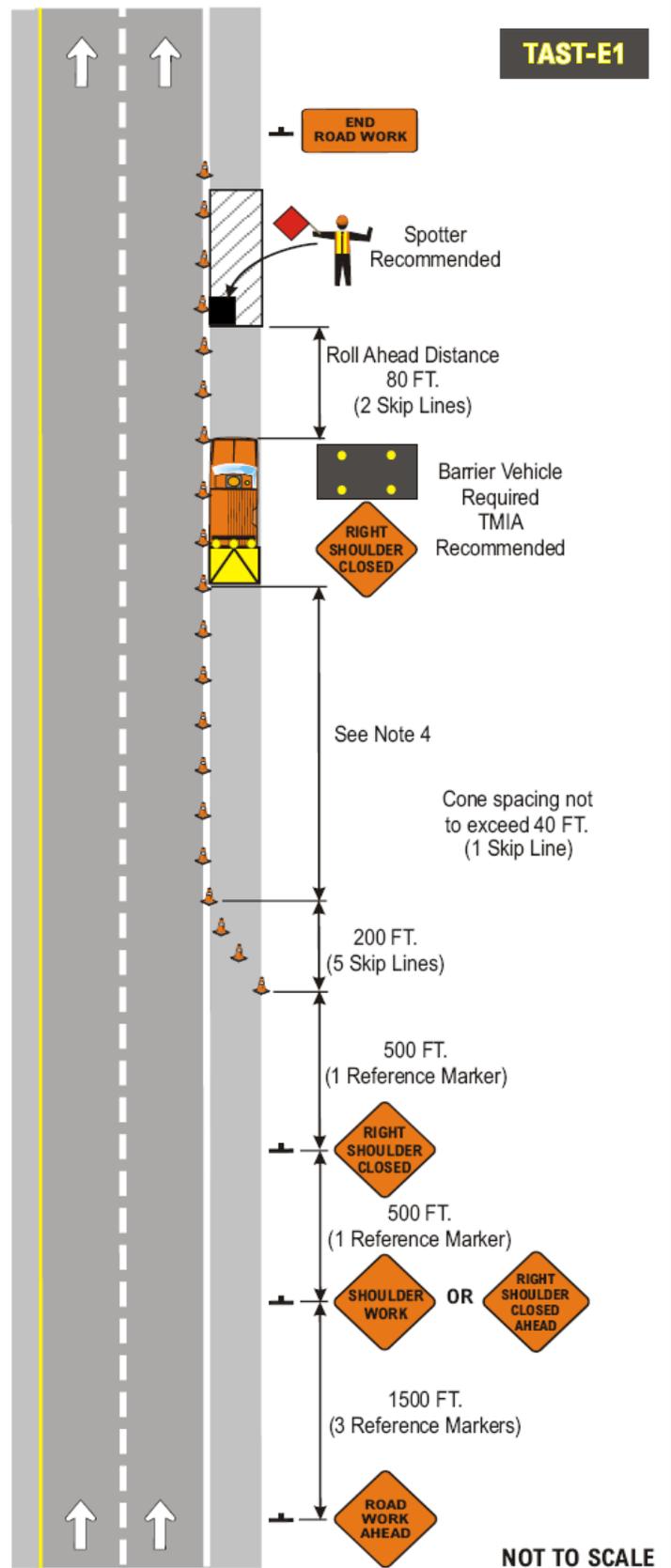
| | | |
|--|----------------------------|---|
| | G20-2 48x24 in. | This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area. |
| | W21-5aR 48x48 in. | |
| | | W20-1 48x48 in. |
| | W21-5 48x48 in. | |
| | | W21-5bR 48x48 in. |
| | Work Area | |
| | Arrow Panel (Caution Mode) | |
| | Barrier Vehicle with TMIA | |

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
SHOULDER CLOSURE
(PAVED SHOULDER 8 FT. OR WIDER)
ON
FREEWAY OR EXPRESSWAY**

AUGUST 2008

TAST-E1



NOT TO SCALE

FREEWAY OR EXPRESSWAY

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.

| Speed Limit (Mph) | Taper Lengths based on Lane Shift | | |
|-------------------|-----------------------------------|------|------|
| | 10' | 11' | 12' |
| 55 | 550' | 605' | 660' |
| 60 | 600' | 660' | 720' |
| 65 | 650' | 715' | 760' |

END ROAD WORK G20-2 48x24 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

LANE CLOSED NYW8-33 48x24 in. **ROAD WORK 1 MILE** W20-1 48x48 in.

RIGHT LANE CLOSED 1/2 MILE W20-5R 48x48 in. **LANE CLOSED** W4-2R 48x48 in.

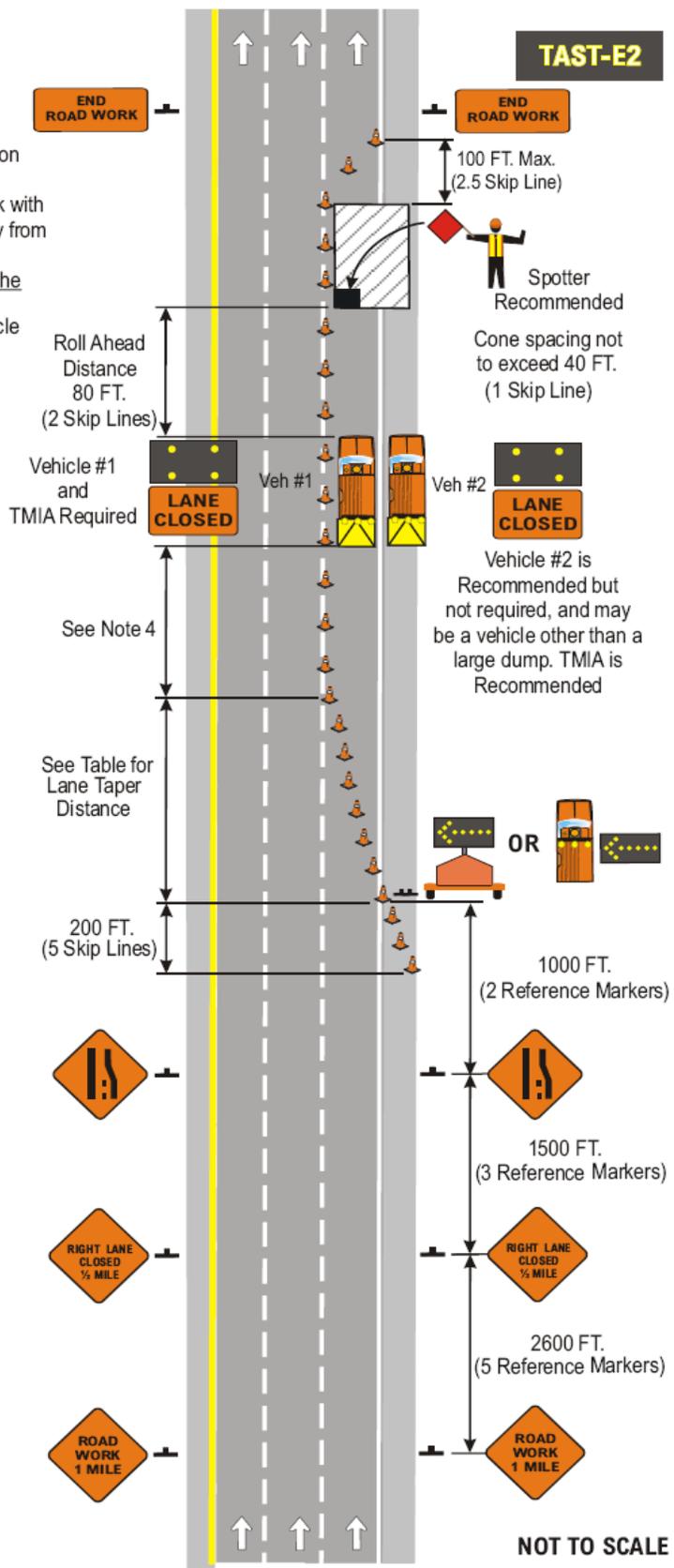
- Work Area
- Arrow Panel (Caution Mode)
- Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT LANE CLOSURE
(PAVED SHOULDER 8 FT. OR WIDER)
ON
FREEWAY OR EXPRESSWAY**

AUGUST 2008

TAST-E2



**FREEWAY OR EXPRESSWAY
PAVED SHOULDER LESS THAN 8 FT.**

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.

| Speed Limit (Mph) | Taper Lengths based on Lane Shift | | | Shoulder Taper based on 4'-6' Shift |
|-------------------|-----------------------------------|------|------|-------------------------------------|
| | 10' | 11' | 12' | |
| 55 | 550' | 605' | 660' | 75'-110' |
| 60 | 600' | 660' | 720' | 80'-120' |
| 65 | 650' | 715' | 760' | 90'-130' |

END ROAD WORK G20-2 48x24 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

LANE CLOSED NYW8-33 48x24 in. **ROAD WORK 1 MILE** W20-1 48x48 in.

RIGHT LANE CLOSED 1/2 MILE W20-5R 48x48 in. **RIGHT LANE CLOSED 1/2 MILE** W4-2R 48x48 in.

 Work Area

 Arrow Panel (Caution Mode)

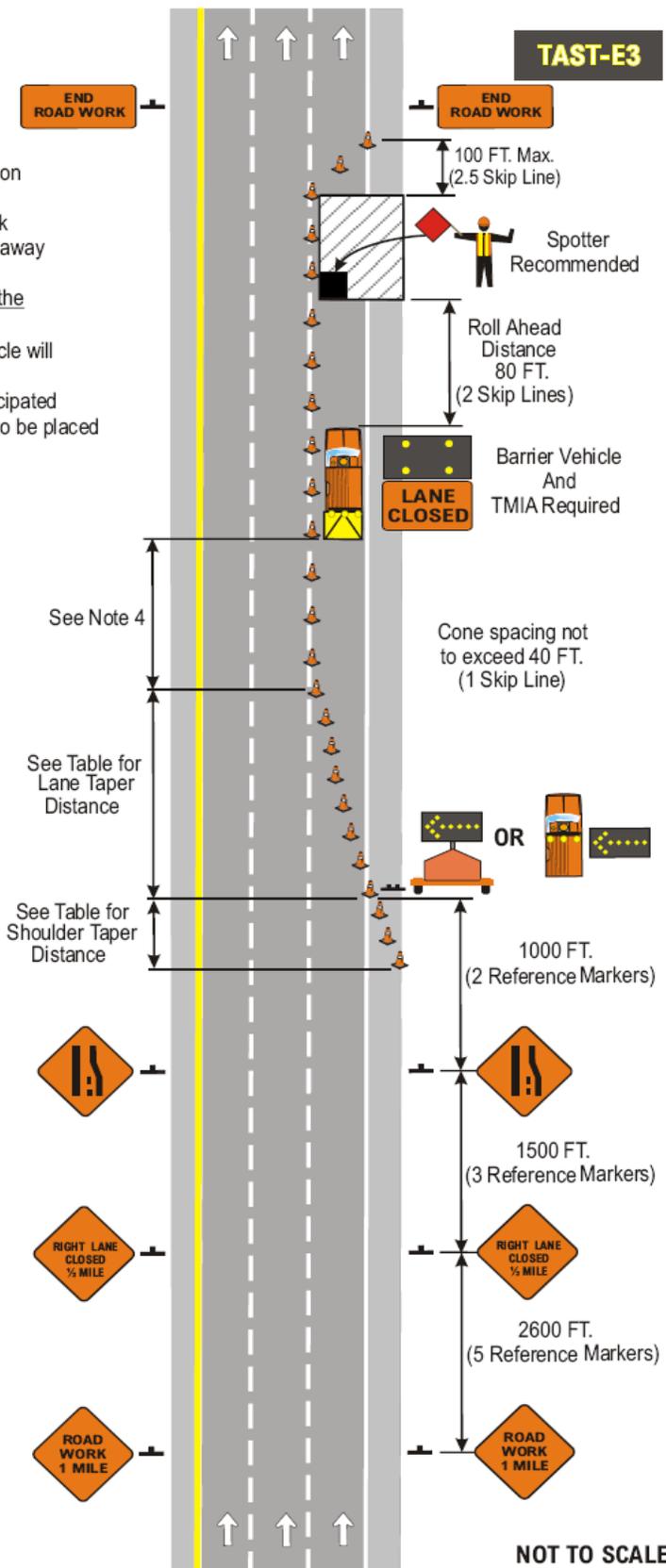
 Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT LANE CLOSURE
(PAVED SHOULDER LESS THAN 8 FT.)
ON
FREEWAY OR EXPRESSWAY**

AUGUST 2008

TAST-E3



FREEWAY OR EXPRESSWAY

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.

| Speed Limit (Mph) | Taper Lengths based on Lane Shift | | |
|-------------------|-----------------------------------|------|------|
| | 10' | 11' | 12' |
| 55 | 550' | 605' | 660' |
| 60 | 600' | 660' | 720' |
| 65 | 650' | 715' | 760' |

LANE CLOSED NYW8-33 48x24 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

END ROAD WORK G20-2 48x24 in.

LANE ENDS MERGE LEFT W9-2L 48x48 in. **ROAD WORK 1 MILE** W20-1 48x48 in.

RIGHT TWO LANES CLOSED 1/2 MILE W20-5aR 48x48 in. **LANE ENDS MERGE LEFT** W4-2R 48x48 in.

Arrow Panel (Caution Mode)

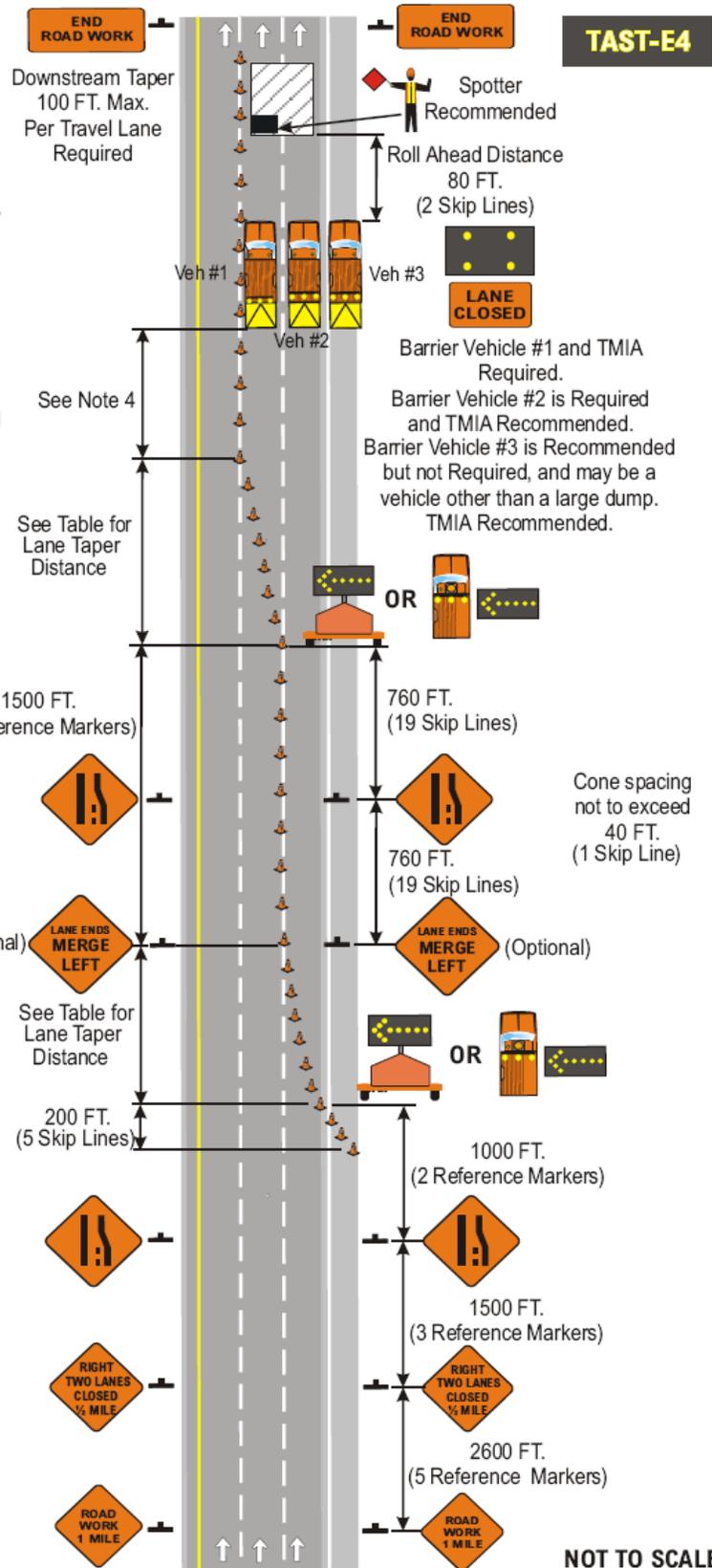
Barrier Vehicle with TMIA **Work Area**

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT TWO LANE CLOSURE
(PAVED SHOULDER 8 FT. OR WIDER)
ON
FREEWAY OR EXPRESSWAY**

AUGUST 2008

TAST-E4



**FREEWAY OR EXPRESSWAY
PAVED SHOULDER LESS THAN 8 FT.**

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.

| Speed Limit (Mph) | Taper Lengths based on Lane Shift | | | Shoulder Taper based on 4'-6' Shift |
|-------------------|-----------------------------------|------|------|-------------------------------------|
| | 10' | 11' | 12' | |
| 55 | 550' | 605' | 660' | 75'-110' |
| 60 | 600' | 660' | 720' | 80'-120' |
| 65 | 650' | 715' | 760' | 90'-130' |

LANE CLOSED NYW8-33
48x24 in.

END ROAD WORK G20-2
48x24 in.

This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

LANE ENDS MERGE LEFT W9-2L
48x48 in.

ROAD WORK 1 MILE W20-1
48x48 in.

RIGHT TWO LANES CLOSED 1/2 MILE W20-5aR
48x48 in.

LANE CLOSED W4-2R
48x48 in.

Arrow Panel (Caution Mode)

Barrier Vehicle with TMIA

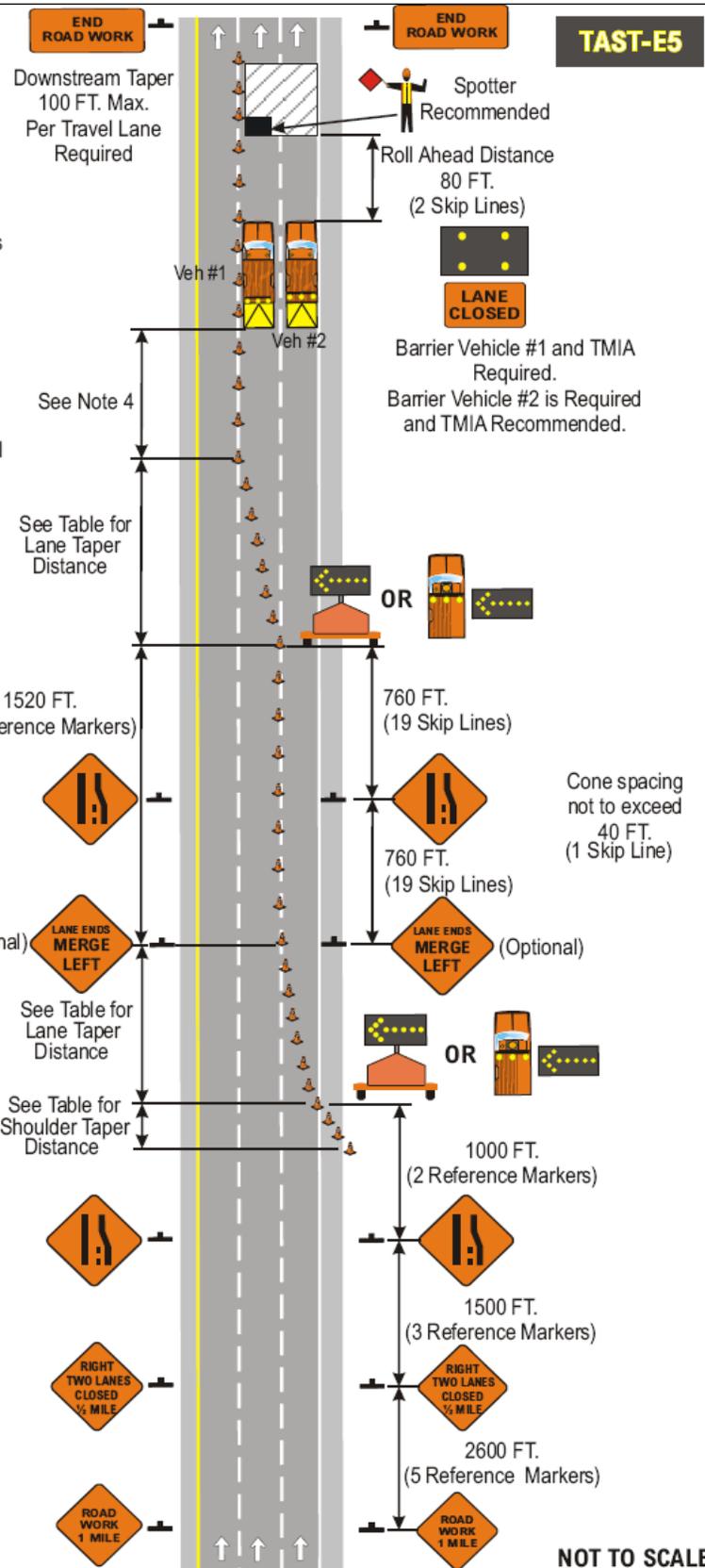
Work Area

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
RIGHT TWO LANE CLOSURE
(PAVED SHOULDER LESS THAN 8 FT.)
ON
FREEWAY OR EXPRESSWAY**

MARCH 2009

TAST-E5



NOT TO SCALE

FREEWAY OR EXPRESSWAY

Notes:

1. Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
2. The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
3. There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
4. Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
5. State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.

| Speed Limit (Mph) | Taper Lengths based on Lane Shift | | | Shoulder Taper based on 4'-6' Shift |
|-------------------|-----------------------------------|------|------|-------------------------------------|
| | 10' | 11' | 12' | |
| 55 | 550' | 605' | 660' | 75'-110' |
| 60 | 600' | 660' | 720' | 80'-120' |
| 65 | 650' | 715' | 760' | 90'-130' |

END ROAD WORK G20-2 48x24 in. This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

LANE CLOSED NYW8-33 48x24 in. **ROAD WORK 1 MILE** W20-1 48x48 in.

LEFT LANE CLOSED 1/2 MILE W20-5L 48x48 in. **ROAD WORK 1/2 MILE** W4-2L 48x48 in.

 Work Area

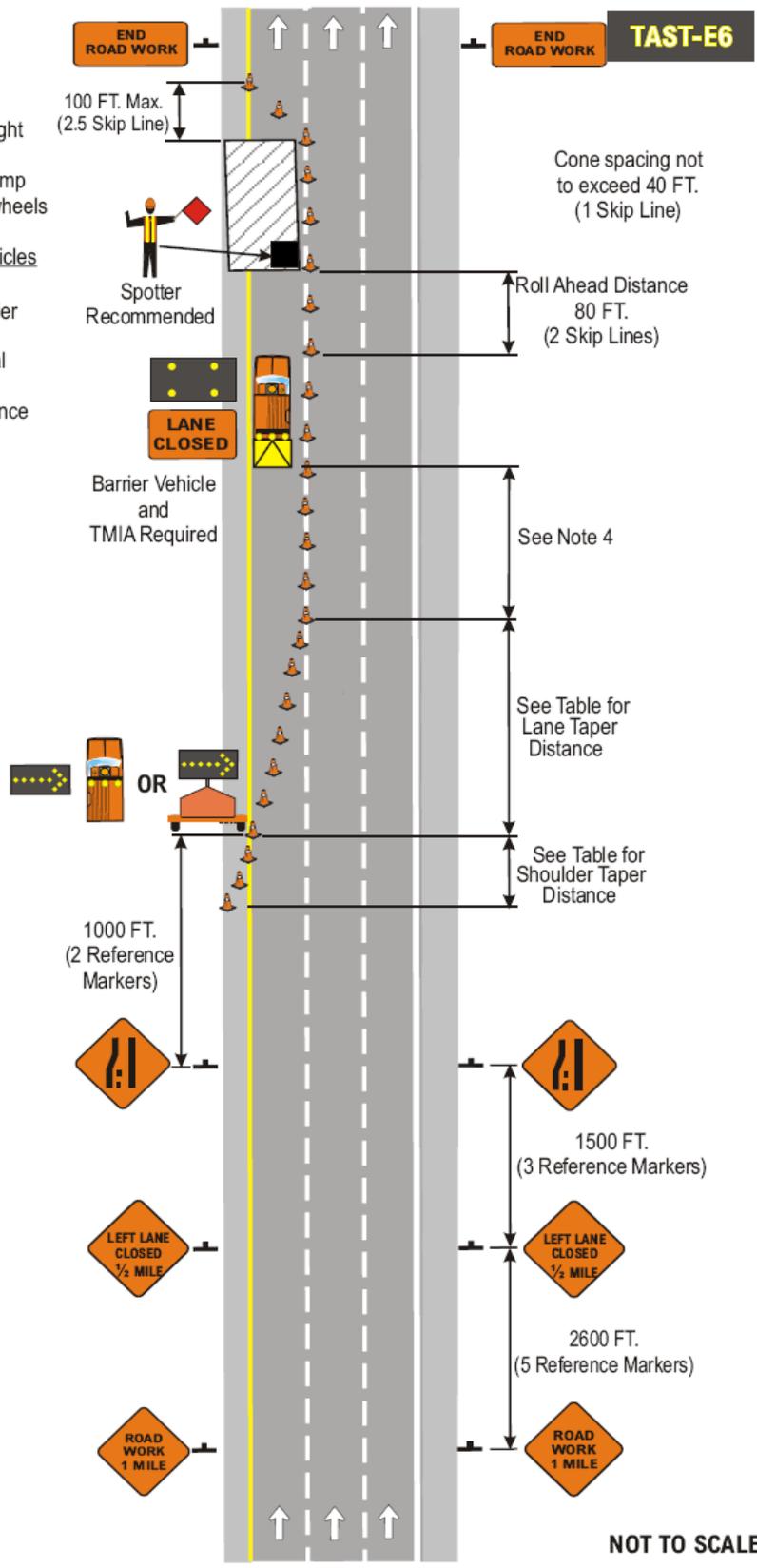
 Arrow Panel (Caution Mode)

 Barrier Vehicle with TMIA

NYSDOT
WORK ZONE TRAFFIC CONTROL

SHORT-TERM STATIONARY
OPERATION INVOLVING
LEFT LANE CLOSURE
(PAVED SHOULDER LESS THAN 8 FT.)
ON
FREEWAY OR EXPRESSWAY

AUGUST 2008 **TAST-E6**



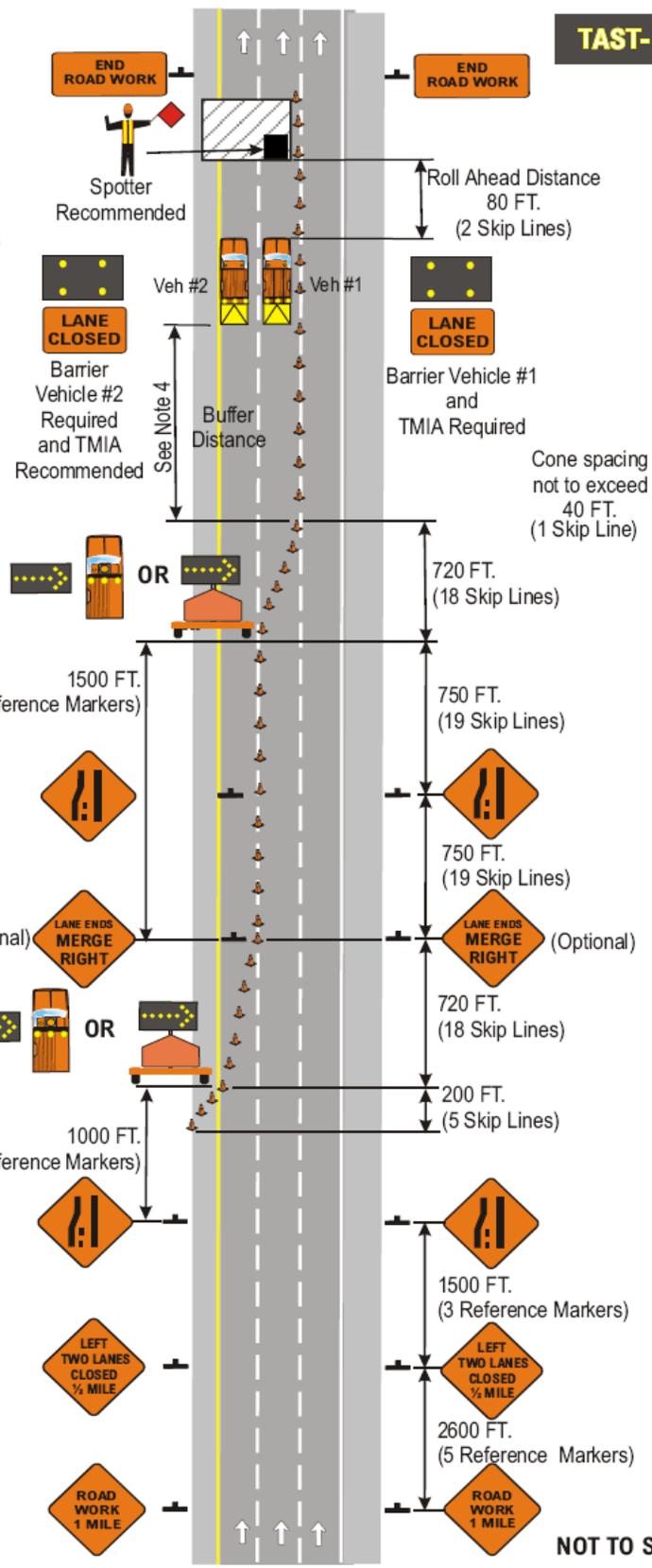
NOT TO SCALE

FREEWAY OR EXPRESSWAY

TAST-E7

Notes:

- Short-term stationary is daytime work that occupies a location for more than 1 hour within a single daylight period.
- The barrier vehicle shall be an unoccupied large dump truck with the parking brake set and with the front wheels turned away from the employees in the work area.
- There shall be no workers, equipment, or other vehicles in the buffer space or the roll ahead distance.
- Any buffer distance provided in advance of the barrier vehicle will add to the safety of the work area.
- State Law signs (NYR9-11 or NYR9-12) are optional if anticipated work duration is less than 4 hours. If used, State Law sign to be placed 1000 FT. in advance of initial warning sign.



LANE CLOSED NYW8-33
48x24 in.

END ROAD WORK G20-2
48x24 in.

This sign shall be located a Maximum distance of 500' (12 Skip Lines) past the work area.

LANE ENDS MERGE RIGHT W9-2R
48x48 in.

ROAD WORK 1 MILE W20-1
48x48 in.

LEFT TWO LANES CLOSED 1/2 MILE W20-5aL
48x48 in.

LANE ENDS MERGE RIGHT (Optional) W4-2L
48x48 in.



Work Area



Arrow Panel (Caution Mode)



Barrier Vehicle with TMIA

**NYSDOT
WORK ZONE TRAFFIC CONTROL**

**SHORT-TERM STATIONARY
OPERATION INVOLVING
LEFT TWO LANE CLOSURE
(PAVED SHOULDER LESS THAN 8 FT.)
ON
FREEWAY OR EXPRESSWAY**

AUGUST 2008

TAST-E7

NOT TO SCALE

WORK ZONE TRAFFIC CONTROL: (Cont'd)

The Contractor shall provide construction signs as specified in Section 619-1 through 619-3 of the Standard Specifications and in the MUTCD. At a minimum the Contractor shall install the following permanent construction signs:

| SIGN | MINIMUM SIZE | LOCATION |
|-------------------------------|---|--|
| ROAD WORK NEXT _____ MILES | <u>G20-1</u> 36" x 18" (900 mm x 450 mm) for conventional, 48" x 24" (1,200 x 600 mm) for freeways | On main line upstream of project in each direction |
| END ROAD WORK | <u>G20-2</u> 36" x 18" (900 mm x 450 mm) for conventional, 48" x 24" (1,200 x 600 mm) for freeways | On main line after end of project in each direction |
| ROAD WORK 500 FT. | <u>W20-1</u> 36" x 36" (900 mm x 900 mm) for conventional , 48" x 48" (1,200 x 1,200 mm) for freeways | On main line 500 feet in advance of the affected highway segment in each direction and on major intersecting roads 300 -500 feet in advance of main line. Sign should be covered if it conflicts with temporary signing in the vicinity. |
| DO NOT PASS | <u>R4-1G</u> 24" x 30" (600 x 750 mm) for conventional | Spaced every 1,000 feet along project in each direction (if centerline tracks are used instead of temporary pavement markings) |
| NO CENTER STRIPE | <u>W8-12</u> 36" x 36" (900 mm x 900 mm) for conventional | On mainline spaced every 2 miles along project in each direction and after every major intersecting road (if centerline tracks are used instead of temporary pavement markings) |
| LOW SHOULDER | <u>W8-9</u> 30" x 30" (750 x 750 mm) for conventional, 48" x 48" (1,200 x 1,200 mm) for freeways | On mainline spaced every 2 miles along project in each direction and after every major intersecting road (if centerline tracks are used instead of temporary pavement markings) |
| GROOVED PAVEMENT | <u>NYW4-5</u> 30" x 30" (750 x 750 mm) for conventional, 48" x 48" (1,200 x 1,200 mm) for freeways | On any roadway 500 feet in advance of rebates milled under this contract, but not paved. Remove or cover after paving rebate. |

Major intersecting roads are defined as through State, County, Town, Village, or City roads. The Contractor may provide Portable signs as shown in Figure 6F-2 of the MUTCD and meeting the requirements of Section 619 of the Standard Specifications for lane closures during work hours.

With prior permission of the State's Resident Engineer, the vendor may provide portable signs as shown in Figure 6F-2 of the MUTCD for the above referenced DO NOT PASS and NO CENTER STRIPE signs. Signs left active at night shall be rigid and reflectorized in accordance with the Standard Specifications. The vendor shall be responsible for assuring that these signs will be in their upright, visible positions twenty-four hours a day, seven days a week while centerline tracks are used instead of pavement markings.

WORK ZONE TRAFFIC CONTROL: (Cont'd)

Special Note - Temporary Pavement Markings:

The Contractor shall install and maintain temporary pavement markings on any paved surface without permanent pavement markings before opening it to traffic, before nightfall or before the end of the work day, whichever comes soonest except for areas that are open during the work shift with channelizing devices or flaggers. Temporary pavement markings shall meet the requirements of Section 619 of the Standard Specifications. As indicated in Section 619-3.06 C., two-lane, two way highways may be left unmarked for a maximum of 3 calendar days provided that NO CENTER STRIPE (W8-12) and DO NOT PASS (R4-1) signs are used in conjunction with centerline tracks installed on a 40 ft cycle to delineate the centerline location.

The State is responsible for the final pavement markings. If the Contractor chooses to install NO CENTER STRIPE and DO NOT PASS signs and centerline tracks instead of Temporary Pavement Markings, the signs shall be left in place for a minimum of 3 calendar days after the project has been completed or until the state has completed installing the final pavement markings, whichever comes first.

All costs for Work Zone Traffic Control including flagging, temporary pavement markings, and construction signs are to be included in the prices bid per metric ton for the bituminous concrete.

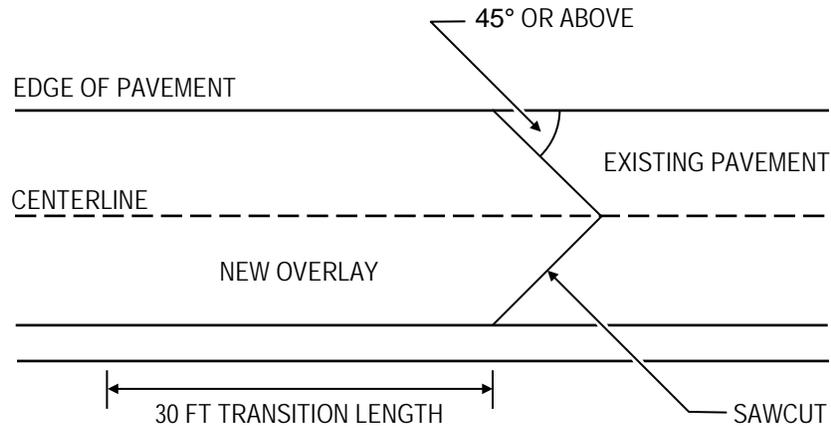
HOT MIX ASPHALT OVERLAY SPLICE (REBATE):

The vendor shall install hot mix asphalt overlay splices (pavement terminations) as per the Detail of Hot Mix Asphalt Overlay Splice. Hot mix asphalt overlay splices shall be installed at the areas indicated in the Location Table for Hot Mix Asphalt Overlay Splices. The cost for sawcutting, milling rebates and cleaning pavement in the splice area shall be included in the price bid per ton of bituminous concrete. Tack coat shall be paid under its own item as specified elsewhere. No separate payments shall be made for hot mix asphalt overlay splices.

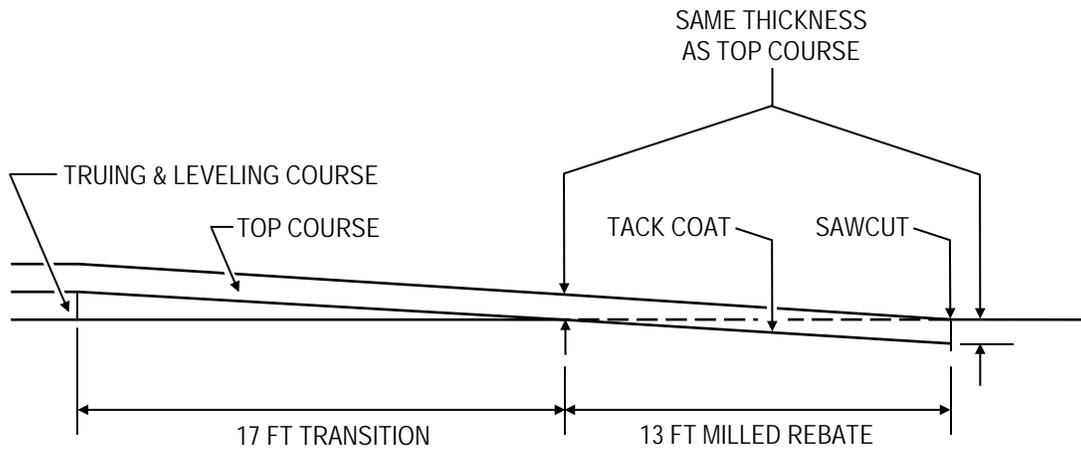
Immediately after the hot mix asphalt overlay splices are milled a temporary asphalt ramp shall be constructed. A cone or drum shall be installed at the ramp and be equipped with a Type A flashing warning light if left in place at night. No separate payment shall be made for the ramps. The cost shall be included in the price bid per ton of bituminous concrete.

Where rebates are milled and ramps are constructed and traffic is to ride on the milled pavement for more than the one work day in which the rebate is milled, GROOVED PAVEMENT signs (NYW4-5) shall be installed on the right side of the roadway, 500 feet upstream of the rebate location. No separate payment shall be made for the GROOVED PAVEMENT sign. The cost shall be included in the price bid per ton of bituminous concrete.

DETAIL OF HOT MIX ASPHALT OVERLAY SPLICE



PLAN



SECTION

Special Note: WORK ZONE INTRUSION INITIATIVE

As part of the Department of Transportation's Work Zone Intrusion Initiative, the following countermeasures shall apply to this Contract Award.

Channelizing Device Spacing Reduction

A maximum channelizing device spacing of 40 feet shall be provided at stationary work sites where workers are exposed to traffic. This spacing shall be maintained a reasonable distance upstream of workers, and shall be used throughout the work zone.

Where tapers are located less than 500 feet from the work site, the 40 foot spacing shall be used in the taper as well.

Drums or vertical panels are preferred for long-term stationary and intermediate-term stationary work zones, and at any locations where the risk of intrusion is high. Traffic cones are normally adequate for work zones set up and removed on a daily basis.

In long lane or shoulder closures, at least two channelizing devices shall be placed transversely at maximum 800 foot intervals to discourage traffic from driving through the closed lane.

Frequent checks shall be made to reset channelizing devices dislodged by traffic.

Flagger Station Enhanced Setups

Additional cones and a flag tree meeting section 6F.57 of the MUTCD shall be used upstream of flagger stations to provide added warning to drivers. These devices shall be used for flagger stations except those that are constantly moving or are in use at one location for no more than a few minutes. If the W20-7a Flagger sign is required, the additional cones and flag tree shall also be used.

For additional details on Flagger Station Enhanced Setups, see Work Zone Traffic Control drawings in this Invitation for Bids.

Temporary Rumble Strips:

The Contractor shall apply temporary rumble strips at the beginning of the work zone in each direction of travel according to the specification below. The Contractor may use either Raised Asphalt Rumble Strips or Raised Removable Tape Rumble Strips.

DESCRIPTION:

This work shall consist of the installation, maintenance and subsequent removal of temporary rumble strips in paving work zones at the locations indicated in the Invitation for Bids or as directed by the Engineer.

MATERIALS:

Rumble strips shall be either constructed in place from a raised strip of asphalt concrete or constructed in place with removable pavement marking tape.

Raised removable tape rumble strips shall be formed from black non-reflectorized removable pavement marking tape.

Raised asphalt rumble strips shall be formed from hot mix asphalt meeting the requirements of Items 402.058902 or 402.098902. Tack coat meeting the requirements of Materials Designation 702-90 Asphalt Emulsion Tack Coat shall be used to adhere the rumble strip to the existing pavement.

Raised Asphalt Rumble Strips:

The roadway surface on which the rumble strips are to be attached shall be dry, free of surface contaminants such as dust or oil, and shall be 45°F or greater unless otherwise authorized by the Engineer. The pavement surface shall be cleaned with compressed air just prior to tack coating and subsequent installation of rumble strips.

Temporary rumble strips shall be placed in a succession of three 6 Strip Patterns according to the attached "Suggested Layout Details - Temporary Rumble Strips". Each strip shall be placed on 10 foot centers and traversing the full width of each travel lane. On curbed roadways, rumble strips shall end a minimum of 3 feet from the curb so as to not interfere with drainage. Rumble strips shall be between 6 inches and 9 inches in width and have a final compacted thickness of 0.4 inches \pm 0.1 inches.

Temporary rumble strips shall be formed using a specially constructed rumble strip paver (drag box) pulled transversely across the pavement, or by hand placement between forms fixed to the pavement. If forms are used, they shall be removed prior to compaction of the asphalt mixture. Compaction shall be accomplished using a plate tamper or a static roller.

MATERIALS: (Cont'd.)

Raised Asphalt Rumble Strips: (Cont'd.)

Any raised rumble strips that fail to adhere to the pavement, or become damaged or flattened such that, in the opinion of the Engineer, they are no longer performing their intended function, shall be replaced or repaired by the Contractor to the satisfaction of the Engineer. Any associated damage to the pavement shall also be repaired by the Contractor to the satisfaction of the Engineer. These replacements or repairs shall be made at no additional expense to the Purchasing Agency.

When directed by the Engineer, (e.g., prior to the start of the winter plowing season), or prior to the placement of successive pavement courses, the Contractor shall completely remove the rumble strips from the pavement. Rumble strips shall be removed upon completion of work and concurrently with the removal of other temporary traffic control signs and devices. Any pavement that is damaged in the process of removing the rumble strips shall be repaired by the Contractor to the satisfaction of the Engineer at no additional expense to the Purchasing Agency.

Raised Removable Tape Rumble Strips:

The rumble strips shall be formed by applying four layers of removable black preformed pavement marking tape. The tape shall be applied to a clean, dry pavement surface in accordance with the manufacturer's recommendations. The pavement surface shall be cleaned with compressed air just prior to application of the tape.

Temporary rumble strips shall be placed in a succession of three 6 Strip Patterns according to the attached "Suggested Layout Details - Temporary Rumble Strips". Each strip shall be placed on 10 foot centers and traversing the full width of each travel lane. On curbed roadways, rumble strips shall end a minimum of 3 feet from the curb so as to not interfere with drainage. Sufficient layers of tape shall be applied such that each finished rumble strip has a thickness of .4 inches \pm .1 inches and is between 6 inches and 9 inches in width.

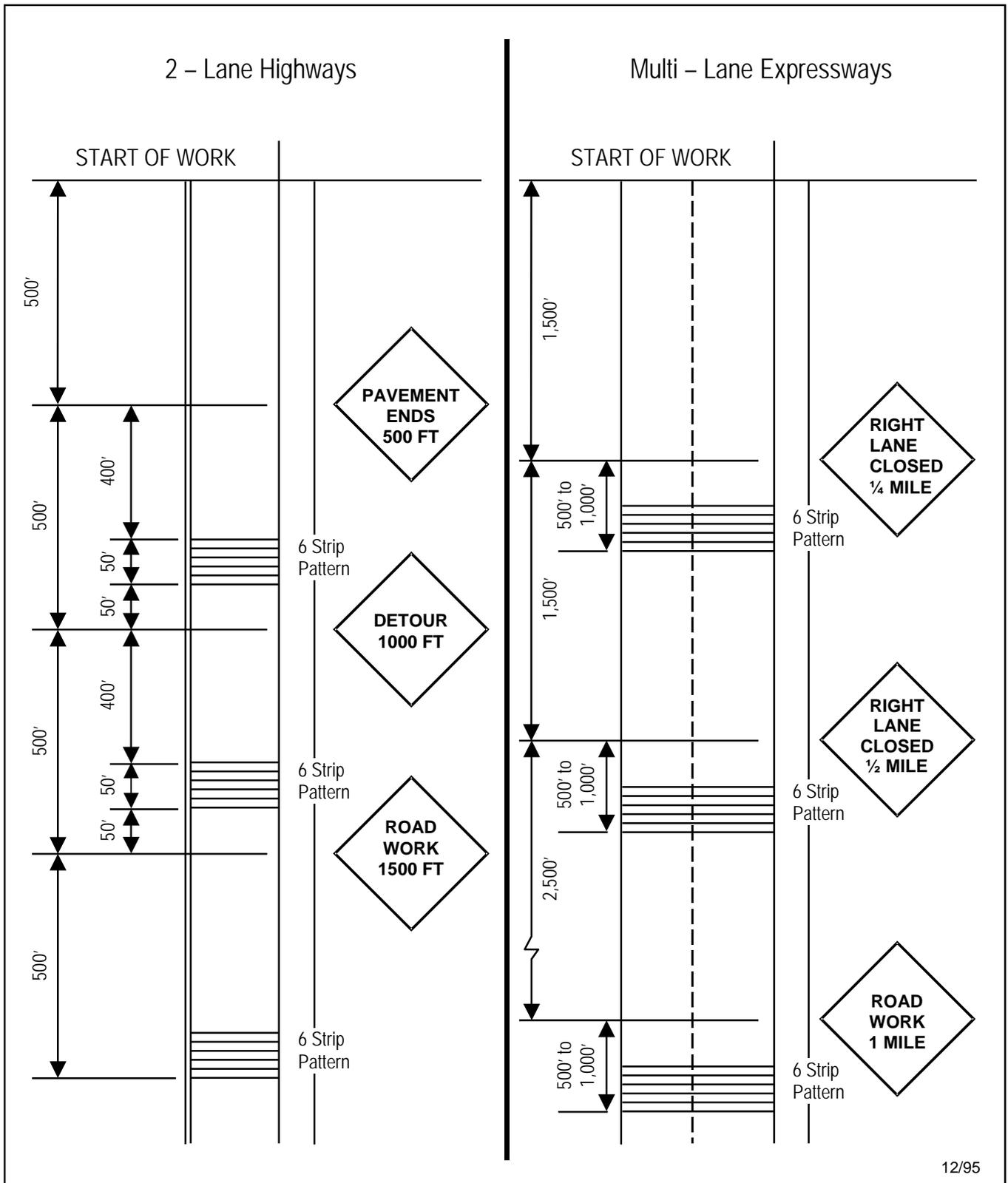
Any raised rumble strips that fail to adhere to the pavement, or become damaged or flattened such that, in the opinion of the Engineer, they are no longer performing their intended function, shall be replaced or repaired by the Contractor to the satisfaction of the Engineer. Any associated damage to the pavement shall also be repaired by the Contractor to the satisfaction of the Engineer. These replacements or repairs shall be made at no additional expense to the Purchasing Agency.

When directed by the Engineer, (e.g. prior to the start of the winter plowing season), or prior to the placement of successive pavement courses, the Contractor shall completely remove the rumble strips from the pavement. Rumble strips shall be removed upon completion of work and concurrently with the removal of other temporary traffic control signs and devices. Any pavement that is damaged in the process of removing the rumble strips shall be repaired by the Contractor to the satisfaction of the Engineer at no additional expense to the Purchasing Agency.

BASIS OF PAYMENT:

All costs for the installation, maintenance and removal of temporary rumble strips are included in the price per ton. No separate payment shall be made.

Suggested Layout Details -- Temporary Rumble Strips



SPECIAL NOTES

FUNDING SOURCE - FEDERAL AID:

Projects 6V1212, 7V1231, 7V1241, 7V1242, 7V1253 are funded by Federal Aid.

FUNDING SOURCE – STATE-FUNDED:

Projects 2V1261, 360267, 360269, 5V1241, 7V1233, 7V1252, 7V1256, and 7V1257 are 100% State funded.

SPECIAL NOTE FOR COORDINATION WITH OTHER PROJECTS

Prior to HMA overlay, Project 6V1213 involves cold recycling through separate contractor(s). These VPP overlay projects require that the paving contractor coordinates their work with corresponding cold recycling contractor to allow required curing period before placing the HMA overlay as well as to minimize disruption to the traveling public and the time traffic is running over a recycled surface.

SPECIAL NOTE – OPTIONAL USE OF WARM MIX ASPHALT (WMA) TECHNOLOGIES

The contractor has the option of using an Approved WMA Technology in the production of all 402, *Hot Mix Asphalt (HMA)* items, except *SUPERPAVE HMA with Ice Retardant items, Waterproofing Bridge Deck HMA items, and Paver-Placed Surface Treatment items*, at no additional cost to the State.

If the contractor chooses to use a WMA technology, the provisions of §401 and §402 shall apply including the following:

Use an approved technology appearing on the Approved List for *Technologies for Warm Mix Asphalt*. Design a mixture using a WMA Technology in accordance with MM 5.16, *Superpave Hot Mix Asphalt Mixture Design and Mixture Verification Procedure*. At a minimum, a one point verification of the mixture’s volumetric properties is acceptable for the following situations:

- When the WMA mix design is based on an existing Production Status HMA mix design.
- When the WMA mix design is based on and utilizes a different WMA technology than an existing Production Status WMA mix design.

Comply with the latest manufacturer’s “Production, Testing, and Compaction Details” from the Approved List for incorporating the WMA technology. Test specimens may be made from plant produced or laboratory prepared WMA. Test specimens must be made from plant produced WMA if adding the WMA technology in the lab does not simulate the production process. The Regional Materials Engineer (RME) may require a State representative be present during the fabrication and testing. Submit the WMA design to the RME for review and verification at least 14 calendar days before production, including:

- Name of WMA technology and the target dosage rate.
- If using an additive other than water,
 - Submit a MSDS for the additive.
 - Submit either enough of the additive for the laboratory mix design verification, or the additive pre-blended in the PG Binder at the correct dosage. If the additive is not pre-blended into the PG Binder, include directions for properly incorporating the additive into the laboratory made mixture.
- Prior to the submission of any mix design, contact the RME to determine if there is an increased concern regarding the mixture’s moisture susceptibility based on the WMA technology and/or the type of aggregate being used, or the performance of similar mixes. The RME may require AASHTO T 283 moisture susceptibility test results, meeting a minimum Tensile Strength Ration (TSR) of 80%, as part of the mix design submission.

Submit Production Quality Control Plan revisions incorporating the WMA technology if not previously submitted.

For 80 Series Compaction Method, complete all breakdown roller passes before the mat temperature falls below 230° F, unless approved by the Director, Materials Bureau.

When the asphalt mixture is being placed over a *Sheet-Applied Waterproofing Membrane*, maintain a minimum delivery temperature in accordance with the Material Detail Sheets prepared by the membrane manufacturer.

SPECIAL NOTE FOR RAILROAD INVOLVEMENT

Contractors are advised that there may be active at-grade railroad crossings within the limits of projects in this Contract Award Notification. The following at-grade railroad crossings have been identified, but there may be others within the limits of these projects that have not been identified:

SPECIAL NOTES (Cont'd)

| Project Number | County | Route | Rail Road Name | Location |
|-----------------------|---------------|--------------|-----------------------|-----------------|
| | | | | |

At the identified at-grade crossings, and any other active at grade railroad crossings encountered on the projects in this Invitation for Bids, the contractor shall conduct its work and handle the equipment such that no part of any material or equipment shall foul a track, catenary, electrical facility or signal facility. A track is fouled when any object is brought within 7.62 M (25') of the centerline of the track or the nearest point of a rail road's catenary, electrical facility or signal facility.

REGION 1 SPECIAL NOTES

Region 1 – Moisture Susceptibility Testing

Any HMA mix design where the primary aggregate component by weight is granite or crushed gravel will be subject to moisture susceptibility testing by the producer during design, unless this requirement is waived by the RME. TSR testing may be required by the RME when there is a change to the asphalt binder source.

Moisture susceptibility will be determined by calculating the tensile strength ratio (TSR) of each specimen according to AASHTO T 283, Resistance of Compacted Asphalt Mixtures to Moisture-Induced Damage, except as modified in Section VI.D. of NYSDOT Materials Method 5.16.

If the TSR of the HMA gyratory specimens is less than 80%, as required in AASHTO M 323, corrective action is required. Corrective action to improve the moisture susceptibility of the HMA mixture can include the use of anti-strip additives or blending of other aggregate materials to reduce the proportion of granite or gravel aggregates in the mix. When corrective action is necessary, any changes made to the design must be noted on the JMF, and all other volumetric and mechanical properties must be evaluated for compliance with NYSDOT Materials Method 5.16 using a one-point design. The results must be reported to the RME prior to production.

Region 1 – Lane Closure Restrictions

All Region 1 Projects shall follow the following holiday restrictions:
There shall be no temporary lane closures permitted on the following dates:

- May 25 – May 29
- July 3 – July 8
- August 31 – September 3
- November 21 – November 25
- December 21 – January 3

REGION 2 SPECIAL NOTES

Region 2 Pavement Markings

It shall be the contractor's responsibility to inventory and document the existing pavement marking patterns prior to milling and/or resurfacing and submit to the Engineer a copy of the inventory prior to beginning work. The contractor shall be responsible for completing all layout work necessary for the installation of all final pavement markings. If the original markings are obliterated, the contractor shall contact the resident engineer for guidance on their location.

2V1261 – Rte 8 – Morgan St. to Babcock Hill Rd.

Item 402.058902 – Type 5 Shim to be used on mainline only. Maximum thickness = ½". Taper out on to shoulder one foot. Use Shim at the following location:

RM 8-2607-1047 to 1082

SPECIAL NOTES (Cont'd)

REGION 4 SPECIAL NOTES

GENERAL:

1. All Truing and leveling courses shall be as indicated in the Superpave Hot Mix Asphalt Design Criteria Table.
2. The contractor shall remove any plowable reflective markers in the pavement, prior to paving. The hole left in the existing pavement, shall then be filled with a hot mix asphalt material; 9.5 mixture, or mixture approved by the Resident Engineer. Cost to be included in the bid price for the associated project.
3. Prior to the start of work, the contractor shall inventory all pavement markings and provide the engineer with a copy of the inventory. The contractor shall be responsible for completing striping layout for each pavement course during the project.
4. Some projects may require loop detectors to be re-established prior to or once paving has been completed. This shall be done by others.
5. The Contractor shall provide traffic protection for all VPP projects in Region 4 in accordance with section 619-01 through 619-03 of the current issue of the NYSDOT Standard Specifications.
6. "DO NOT PASS" signs shall be installed as follows:
 - Signs shall be posted at the beginning of where pavement markings are not present.
 - Spacing between first and second sign shall not exceed 1100 feet.
 - Intermediate signs shall be placed at no larger than 2500 foot increments.
 - Additional signs shall be placed downstream of every intersection.
7. The "NO CENTER STRIPE" sign shall be posted at the beginning of the work zone and repeated at 2-mile intervals in long work zones.
8. The installation of temporary rumble strips at the beginning of each project work zone shall be at the discretion of the engineer.

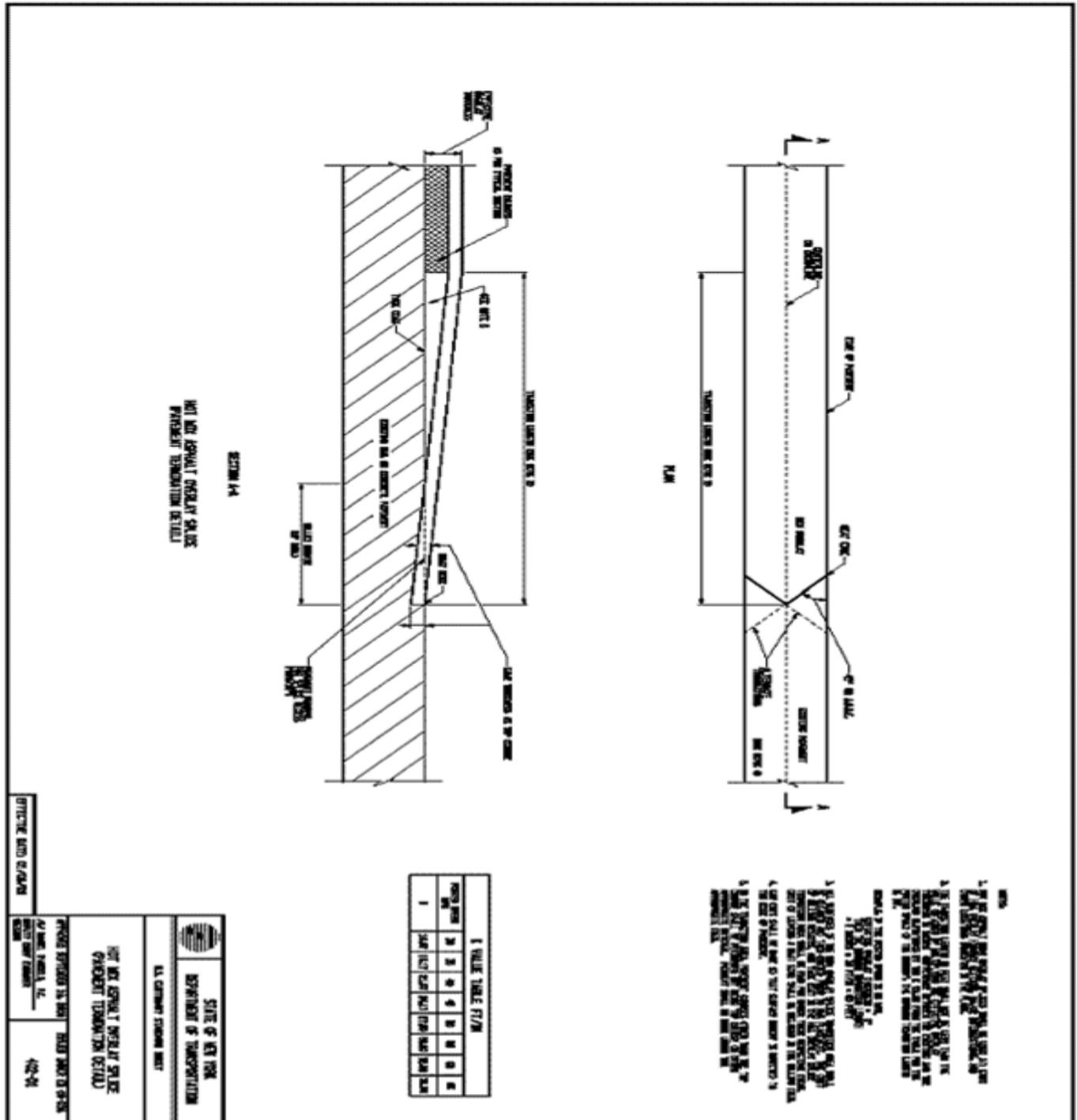
SPECIAL NOTES (Cont'd)

REGION 6 SPECIAL NOTES

Region 6 – Hot Mix Asphalt Overlay Splices (Rebate):

All Region 6 hot mix asphalt overlay splices (pavement terminations) shall be installed as per NYSDOT Standard Sheet 402-01 issued under EB 08-036.

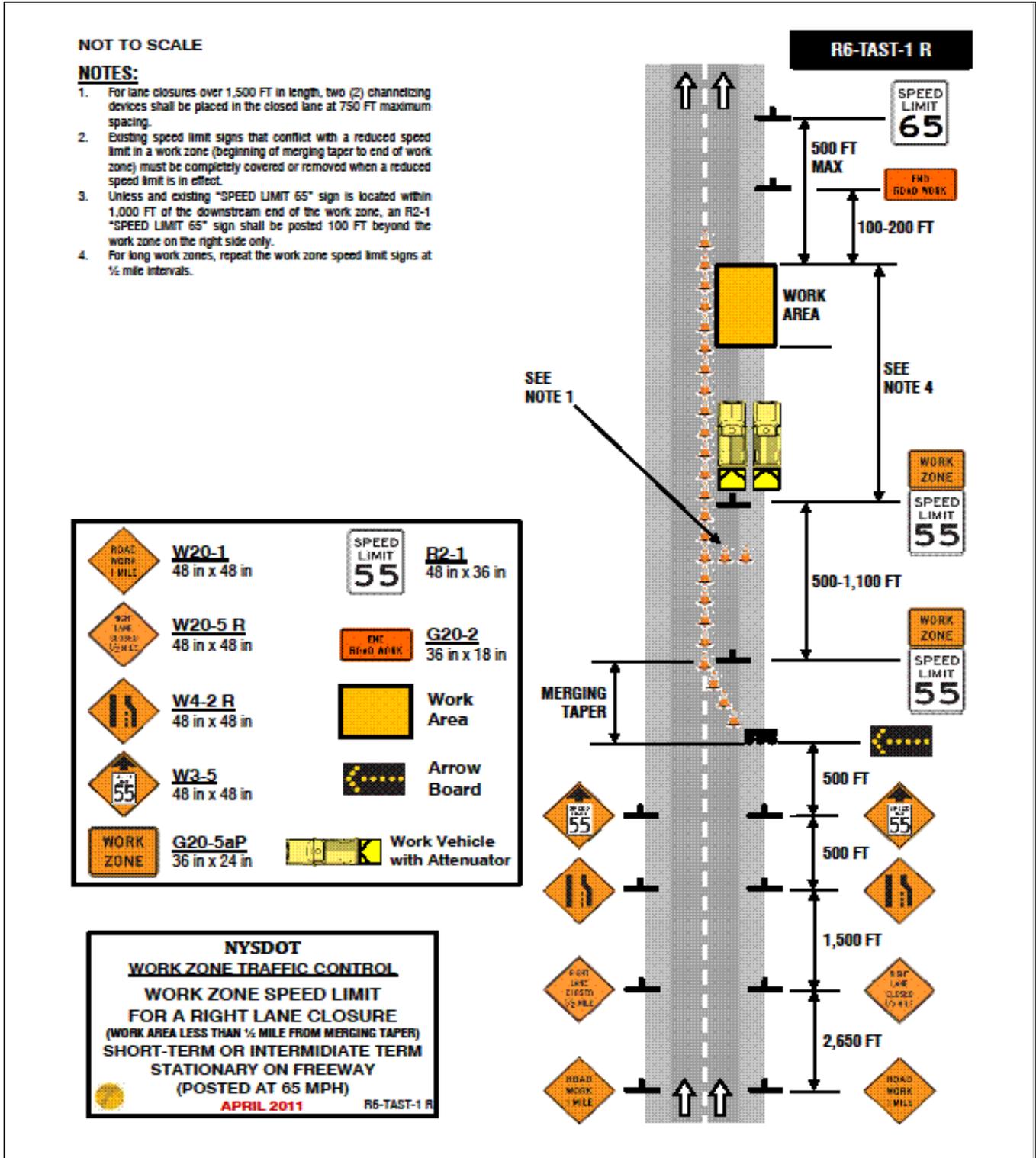
SEE SHEET 402-01 FOR
DETAILS OF HOT MIX ASPHALT
OVERLAY SPICES (REBATE)



SPECIAL NOTES (Cont'd)

Region 6 Special Work Zone Traffic Control on I86 and I390

Lane closures within Region 6 on I86 or I390 shall comply with the details shown in the attached drawings R6-TAST-1R (see below) or R6-TAST-1L (see following page).



SPECIAL NOTES (Cont'd)

Region 6 Special Work Zone Traffic Control on I86 and I390 (Cont'd.)

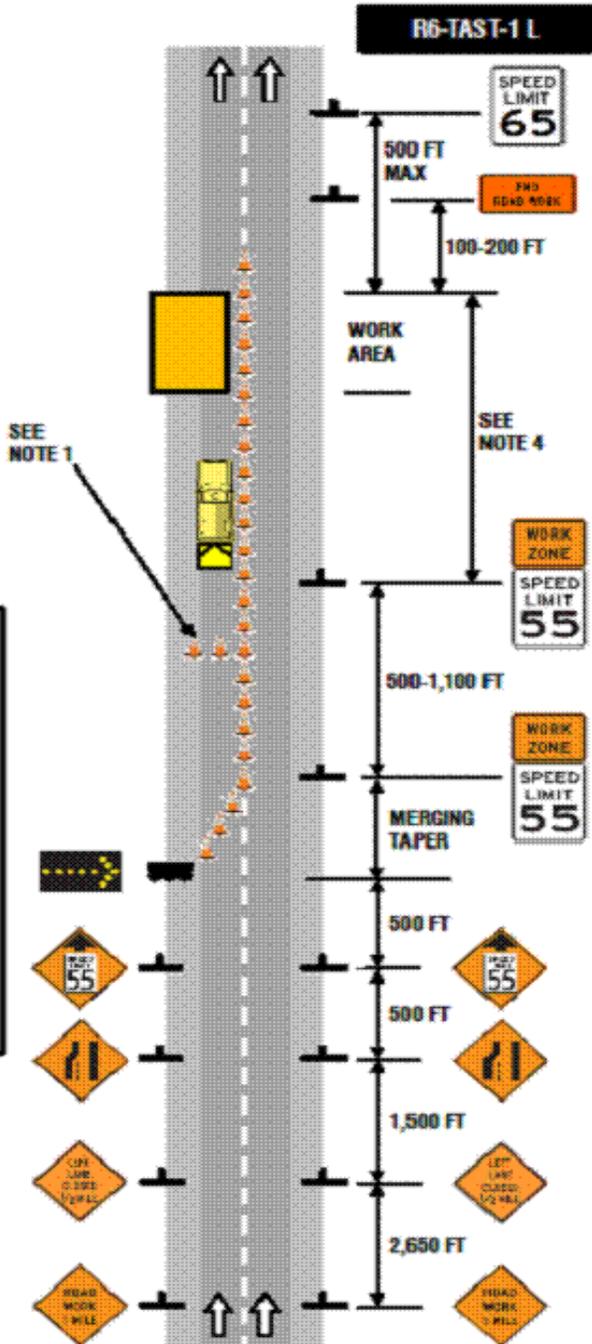
NOT TO SCALE

NOTES:

1. For lane closures over 1,500 FT in length, two (2) channelizing devices shall be placed in the closed lane at 750 FT maximum spacing.
2. Existing speed limit signs that conflict with a reduced speed limit in a work zone (beginning of merging taper to end of work zone) must be completely covered or removed when a reduced speed limit is in effect.
3. Unless an existing "SPEED LIMIT 55" sign is located within 1,000 FT of the downstream end of the work zone, an R2-1 "SPEED LIMIT 55" sign shall be posted 100 FT beyond the work zone on the right side only.
4. For long work zones, repeat the work zone speed limit signs at 1/4 mile intervals.

| | | | |
|--|---------------------------------|--|-------------------------------|
| | W20-1 48 in x 48 in | | R2-1 48 in x 36 in |
| | W20-5 L 48 in x 48 in | | G20-2 36 in x 18 in |
| | W4-2 L 48 in x 48 in | | Work Area |
| | W3-5 48 in x 48 in | | Arrow Board |
| | G20-5aP 36 in x 24 in | | Work Vehicle with Attenuator |

NYSDOT
WORK ZONE TRAFFIC CONTROL
WORK ZONE SPEED LIMIT
FOR A LEFT LANE CLOSURE
(WORK AREA LESS THAN 1/4 MILE FROM MERGING TAPER)
SHORT-TERM OR INTERMEDIATE TERM
STATIONARY ON FREEWAY
(POSTED AT 65 MPH)
APRIL 2011 R6-TAST-1 L



SPECIAL NOTES (Cont'd)

REGION 7 SPECIAL NOTES

Special Work Zone Traffic Control – Pilot Vehicle – Region 7:

Unless otherwise specified, the highway shall be kept open to traffic at all times. Traffic shall be discontinued on the lanes where work is being performed on these projects; and as soon as recycling is done and rolled, controlled traffic may be permitted thereon. For Region 7 projects in this Contract Award Notification, the Contractors shall provide sufficient two-way radio equipped pilot vehicles to guide traffic around recycling work at a speed not to exceed 15 mph. The pilot vehicles shall be equipped with construction signs meeting the requirements of Section 6F.54 of the Manual of Uniform Traffic Control Devices.

| SIGN | MINIMUM SIZE | LOCATION |
|----------------------------|--------------------------------|----------------------------|
| PILOT VEHICLE FOLLOW ME | G20-4Conventional 36" X 18" | ON BACK OF PILOT VEHICLES. |

The pilot vehicle shall have the name of the Contractor prominently displayed.

All cost for Work Zone Traffic Control including flagging, temporary pavement markings, channelizing devices, construction signs, and pilot vehicles is included in the prices per square yard for cold recycling. No separate payment shall be made.

Exposed Longitudinal Joints:

Exposed Longitudinal Joints on any asphalt material placed by the Contractor under this award will not be permitted overnight. All centerline joints and/or abutting travel lane joints shall be required to be closed by the end of each work day.

SUPERPAVE HOT MIX ASPHALT:

The following are design criteria for SUPERPAVE Hot Mix Asphalt Items for projects contained in this Contract Award Notification:

SUPERPAVE HOT MIX ASPHALT DESIGN CRITERIA:

| Project Number | Items | 80kN EASLs | Aggregate Size | PG Binder |
|-----------------------|----------------------------|------------------------|-----------------------|---------------------------|
| 2V1261 | 402.06820118 402.058902 | <30.0 Mil N/A | 6.3 Type 5 Shim | PG 64-22 ER** PG 64-22 |
| 360267 | 402.096202 | <10.0 Mil | 9.5 | PG 64-22 |
| 360269 | 402.096202 402.018902 | <30.0 Mil <30.0 Mil | 9.5 9.5 | PG 64-22 PG 64-22 |
| 5V1241 | 402.096202 | <3.0 Mil | 9.5 | PG 64-22 |
| 6V1212 | 402.06830118 402.018902 | <30.0 Mil <30.0 Mil | 6.3 | PG 64-22 ER** PG 64-22 |
| 6V1213 | 402.06820118 402.018902 | <30.0 Mil <30.0 Mil | 6.3 | PG 64-22 ER** PG 64-22 |
| 7V1231 | 402.096302 | <10.0 Mil | 9.5 | PG 64-22 |
| 7V1233 | 402.096302 | <3.0 Mil | 9.5 | PG 64-22 |
| 7V1241 | 402.096202 | <10.0 Mil | 9.5 | PG 64-22 |
| 7V1242 | 402.06830118 | <30.0 Mil | 6.3 | PG 64-22 ER** |
| 7V1252 | 402.097302 402.018902 | <3.0 Mil <3.0 Mil | 9.5 9.5 | PG 64-22 PG 64-22 |
| 7V1253 | 402.096302 | <3.0 Mil | 9.5 | PG 64-22 |
| 7V1256 | 402.096302 402.018902 | <3.0 Mil <3.0 Mil | 9.5 9.5 | PG 64-22 PG 64-22 |
| 7V1257 | 402.096302 402.018902 | <10.0 Mil <10.0 Mil | 9.5 9.5 | PG 64-22 PG 64-22 |

** Binder shall be polymer modified to meet the elastic recovery requirements shown in the specifications.

PROJECT DIMENSIONS

| Project Number | Items | Resurfacing Depth (in) | Travel Lanes Width (ft) (total) | Lane Width (ft) (one lane) | Shoulder Width (ft) (one shldr) | Number Lanes |
|-----------------------|----------------------------|-------------------------------|--|-----------------------------------|--|---------------------|
| 2V1261 | 402.06820118 402.058902 | 1 0.5 | 24 24 | 12 12 | 8 8 | 2 2 |
| 360267 | 402.096202 | 1.5 | 20 | 10 | 4 | 2 |
| 360269 | 402.096202 402.018902 | 1.5 0.75 | 24 24 | 12 12 | 8 8 | 2 2 |
| 5V1241 | 402.096202 | 1.5 | 22 | 11 | 4 | 2 |
| 6V1212 | 402.06830118 402.018902 | 1 1 | 24 24 | 12 12 | 6 6 | 2 2 |
| 6V1213 | 402.06820118 402.018902 | 1 1 | 24 24 | 12 12 | 7.3 7.3 | 2 2 |
| 7V1231 | 402.096302 | 1.5 | 24 to 40 | 12 | 6 to 10 | 2 & 3 |
| 7V1233 | 402.096302 | 1.5 | 24 to 44 | 12 to 22 | 6 to 8 | 2 |
| 7V1241 | 402.096202 | 1.5 | 40 to 60 | 13 to 15 | 0 | 3 & 4 |
| 7V1242 | 402.06830118 | 0.75 | 22 to 24 | 11 to 12 | 6 to 8 | 2 |
| 7V1252 | 402.097302 402.018902 | 1.5 0.5 | 24 24 | 12 12 | 6.5 6.5 | 2 2 |
| 7V1253 | 402.096302 | 1.5 | 24 | 12 | 6 to 10 | 2 |
| 7V1256 | 402.096302 402.018902 | 1.5 0.5 | 24 24 | 12 12 | 10 10 | 2 2 |
| 7V1257 | 402.096302 402.018902 | 1.5 0.5 | 24 24 | 12 12 | 2 to 10 2 to 10 | 2 2 |

REBATES:

| Project Number | Rebate Location | Rebate Width (Ft) |
|-----------------------|---|--|
| 2V1261 | RM 8-2607-1082, 800' South of Morgan Rd RM 8-2607-1266 & 1263 (BINS 1004590 & 1073890) RM8-2607-1253 (BIN 1004570) RM 8-2607-1047, Babcock Hill Rd | 40 4 at 40 2 at 45 40 |
| 360267 | RM 34 3604 3000 RM 34 3604 3025 | 28 28 |
| 360269 | RM 13 3603 1112 RM 13 3603 1122 | 40 40 |
| 5V1241 | RM 438-5301-1007 South Limit South Approach to BIN 6060710 North Approach to BIN 6060710 RM 438-5301-2062 North Limit | 30 30 30 30 |
| 6V1212 | RM 243 6102 1000 Project Begin RM 243 6102 1040 Project End | 36 36 |
| 6V1213 | RM 19 6101 1173 Project Begin RM 19 6101 1190 RM 19 6101 1197 RM 19 6101 1216 Project End | 40 40 40 38 |
| 7V1231 | RM12-7306-1000 Project Begin RM12-7306-1097 Project End | 65.0 67.9 |
| 7V1233 | No Rebates | |
| 7V1241 | RM26-7406-1257 Project Begin RM26-7406-1265 Project End | 84.9 56.6 |
| 7V1242 | RM26-7406-1304 Project Begin RM26-7307-1004 Project End | 56.6 56.6 |
| 7V1252 | No Rebates | |
| 7V1253 | RM56-7501-1000 Project Begin RM56-7501-1032 Project End | 62.8 53.7 |
| 7V1256 | No Rebates | |
| 7V1257 | No Rebates | |

| OGS/ Project No. | County/Location | Item | Est Tons/ Gallons | Unit Price | NYS DOT Facility | Contractor |
|---------------------------------|--|--------------|----------------------------------|-----------------------|---------------------------------|-------------------------------|
| <u>16</u> | | | | | | |
| 2V1261 | Oneida County Rte 8 | 402.06820118 | 5,000 | \$78.55 | H0038 | Hanson Aggregates NY |
| | Babcock Hill Rd to Morgan St | 402.058902 | 1,700 | \$73.44 | H0038 | |
| | RM 8 2607/ 1047-1082 Town of Paris 3.7 CL Miles | 407.02010018 | 8,500 | \$2.16 | L0204 | |
| <u>26</u> | | | | | | |
| 360267 | Tompkins County Rte 34 | 402.096202 | 3,400 | \$69.95 | H0178 | Broome Bituminous Products |
| | Rte 13 to Cayuga Heights Rd RM 34 3604/ 3000-3025 Towns Ithaca and Lansing 2.5 CL Miles | 407.0101 | 2,036 | \$3.50 | L0903 | |
| <u>28</u> | | | | | | |
| 360269 | Tompkins County Rte 13 | 402.096202 | 1,936 | \$68.00 | H0178 | Broome Bituminous Products |
| | Rte 327 to Rte 13A | 402.018902 | 968 | \$70.00 | H0178 | |
| | RM 13 3603/ 1112-1122 Town of Ithaca 1.0 CL Miles | 407.0101 | 2,318 | \$3.50 | L0903 | |
| <u>48</u> | | | | | | |
| 5V1241 | Erie County Rte 438 | 402.096202 | 8,959 | \$79.77 | H0104 | Milherst Construction |
| | Brant Reservation Rd to Taylor Hollow Rd RM 438 5301/ 1007-2062 Cattaraugus Territory SNI 6.2 CL Miles | 407.0101 | 5,430 | \$4.80 | L0502 | |
| <u>71</u> | | | | | | |
| 6V1212 | Allegany County Route 243 | 402.06830118 | 4,675 | \$83.05 | H0348 | Milherst Construction |
| | Cattaraugus County to Rushford | 402.018902 | 700 | \$84.30 | H0348 | |
| | RM 243 6102/ 1000-1040 Town of Rushford 4.0 CL Miles | 407.02010018 | 5,600 | \$4.70 | L0502 | |

| OGS/ Project No. | County/Location | Item | Est Tons/ Gallons | Unit Price | NYS DOT Facility | Contractor |
|---------------------------------|--|--------------|----------------------------------|-----------------------|---------------------------------|--------------------------|
| <u>74</u> | | | | | | |
| 6V1213 | Allegany County Rte 19 South and North of Belmont RM 19 6101/ 1173-1190 and 1197-1216 Town of Amity and Village of Belmont 3.5 CL Miles | 402.06820118 | 4,450 | \$88.00 | H0348 | Milherst Construction |
| | | 402.018902 | 700 | \$89.60 | H0348 | |
| | | 407.02010018 | 5,300 | \$4.65 | L0502 | |
| <u>99</u> | | | | | | |
| 7V1231 | Jefferson County Rte 12 Lewis County Ln to Gotham St RM 12 7306/ 1000-1097 Towns of Watertown, Rutland, and Champion 9.7 CL Miles | 402.096302 | 23,500 | \$70.36 | H0365/ H0277 | Barrett Paving Materials |
| | | 407.0101 | 12,500 | \$2.10 | L0403 | |
| <u>101</u> | | | | | | |
| 7V1233 | Jefferson County Rte 3 Great Bend to Deferiet RM 3 7302/ 3086.5-3102 Town of Champion 1.6 CL Miles | 402.096302 | 3,400 | \$71.89 | H0365/ H0277 | Barrett Paving Materials |
| | | 407.0101 | 1,850 | \$2.22 | L0403 | |
| <u>103</u> | | | | | | |
| 7V1241 | Lewis County Rte 26 Rte 12 OLAP North State St RM26 7406/ 1257-1265 Village of Lowville 0.8 CL Miles | 402.096202 | 1,967 | \$86.54 | H0133 | Hanson Aggregates NY |
| | | 407.0101 | 1,115 | \$2.00 | L0703 | |

| OGS/ Project No. | County/Location | Item | Est Tons/ Gallons | Unit Price | NYS DOT Facility | Contractor |
|---------------------------------|---|--------------|----------------------------------|-----------------------|---------------------------------|--------------------------|
| <u>104</u> | | | | | | |
| 7V1242 | Lewis County Rte 26 | 402.06830118 | 9,721 | \$78.47 | H0133 | Hanson Aggregates NY |
| | Denmark Town Ln to Rte 126 Intersection RM26 7406/ RM26 7307/ 1304-1397, 1000-1004 Towns of Lowville and Denmark; and Village of West Carthage 9.7 CL Miles | 407.02010018 | 11,018 | \$2.55 | L0703 | |
| <u>107</u> | | | | | | |
| 7V1252 | St Lawrence County Rte 68 | 402.097302 | 1,825 | \$81.70 | H0051 | Hanson Aggregates NY |
| | Rte 37 to Adams Ave RM 68 7501/ 3000-3010 | 402.018902 | 455 | \$77.43 | H0051 | |
| | City of Ogdensburg Town of Oswegatchie 1.0 CL Miles | 407.0101 | 1,815 | \$2.00 | L0703 | |
| <u>108</u> | | | | | | |
| 7V1253 | St Lawrence County Rte. 56 | 402.096302 | 6,300 | \$76.78 | H0137/ H0261 | Barrett Paving Materials |
| | Sevey Corners to Hamm's Inn RM56 7501/ 1000-1032 Town of Colton 3.2 CL Miles | 407.0101 | 3,600 | \$2.10 | L0403 | |
| <u>111</u> | | | | | | |
| 7V1256 | St Lawrence County Rte 3 | 402.096302 | 3,750 | \$80.84 | H0137/ H0261 | Barrett Paving Materials |
| | Star Lake Village RM3 7504/ 1191-1209 | 402.018902 | 750 | \$83.20 | H0137/ H0261 | |
| | Town of Fine 1.8 CL Miles | 407.0101 | 3,380 | \$2.13 | L0403 | |
| <u>112</u> | | | | | | |
| 7V1257 | St Lawrence County Rte 58 | 402.096302 | 2,800 | \$84.36 | H0133 | Hanson Aggregates NY |
| | Gouverneur Village RM58 7501/ 1219-1235 | 402.018902 | 675 | \$81.00 | H0133 | |
| | Village of Gouverneur 1.6 CL Miles | 407.0101 | 2,700 | \$2.00 | L0703 | |

**State of New York
Office of General Services
NEW YORK STATE PROCUREMENT
Contract Performance Report**

Please take a moment to let us know how this contract award has measured up to your expectations. If reporting on more than one contractor or product, please make copies as needed. This office will use the information to improve our contract award, where appropriate. **Comments should include those of the product's end user.**

Contract No.: _____ **Contractor:** _____

Describe Product* Provided (Include Item No., if available): _____

***Note:** "Product" is defined as a deliverable under any Bid or Contract, which may include commodities (including printing), services and/or technology. The term "Product" includes Licensed Software.

| | Excellent | Good | Acceptable | Unacceptable |
|---|-----------|------|------------|--------------|
| • Product meets your needs | | | | |
| • Product meets contract specifications | | | | |
| • Pricing | | | | |

CONTRACTOR

| | Excellent | Good | Acceptable | Unacceptable |
|-------------------------------------|-----------|------|------------|--------------|
| • Timeliness of delivery | | | | |
| • Completeness of order (fill rate) | | | | |
| • Responsiveness to inquiries | | | | |
| • Employee courtesy | | | | |
| • Problem resolution | | | | |

Comments: _____

_____ (over)

Agency: _____ Prepared by: _____

Address: _____ Title: _____

_____ Date: _____

_____ Phone: _____

_____ E-mail: _____

Please detach or photocopy this form & return by FAX to 518/474-2437 or mail to:

OGS NEW YORK STATE PROCUREMENT
Customer Services, 38th Floor
Corning 2nd Tower - Empire State Plaza
Albany, New York 12242
* * * * *